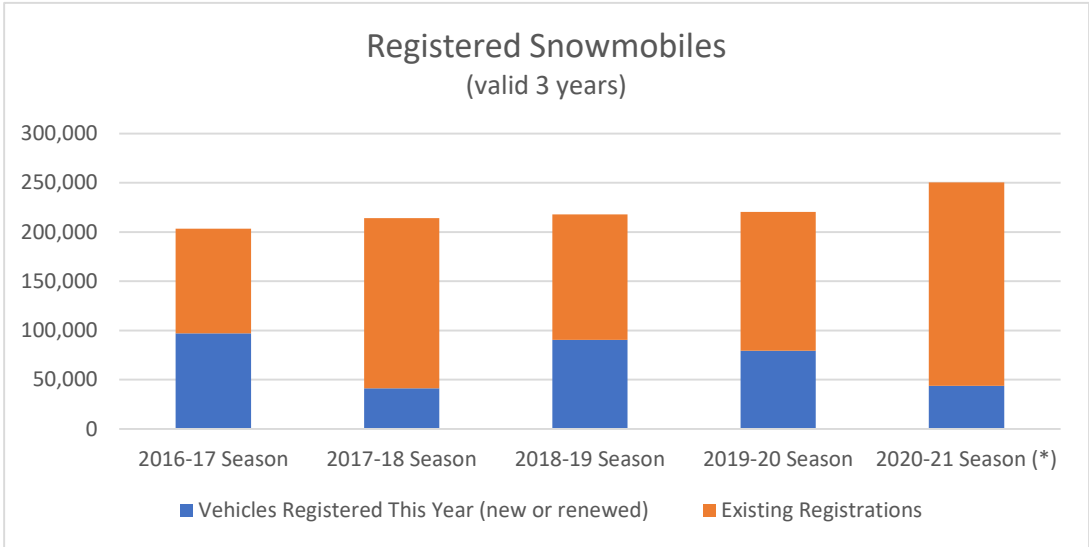


# Snowmobile Vehicle Registrations

	Registered Snowmobiles in Wisconsin				
	2016-17 Season	2017-18 Season	2018-19 Season	2019-20 Season	2020-21 Season (*)
Vehicles Registered This Year (new or renewed)	96,980	41,217	90,415	79,379	43,837
Existing Registrations	106,454	172,952	127,565	140,969	206,730
<b>Total Active Registrations</b>	<b>203,434</b>	<b>214,169</b>	<b>217,980</b>	<b>220,348</b>	<b>250,567</b>

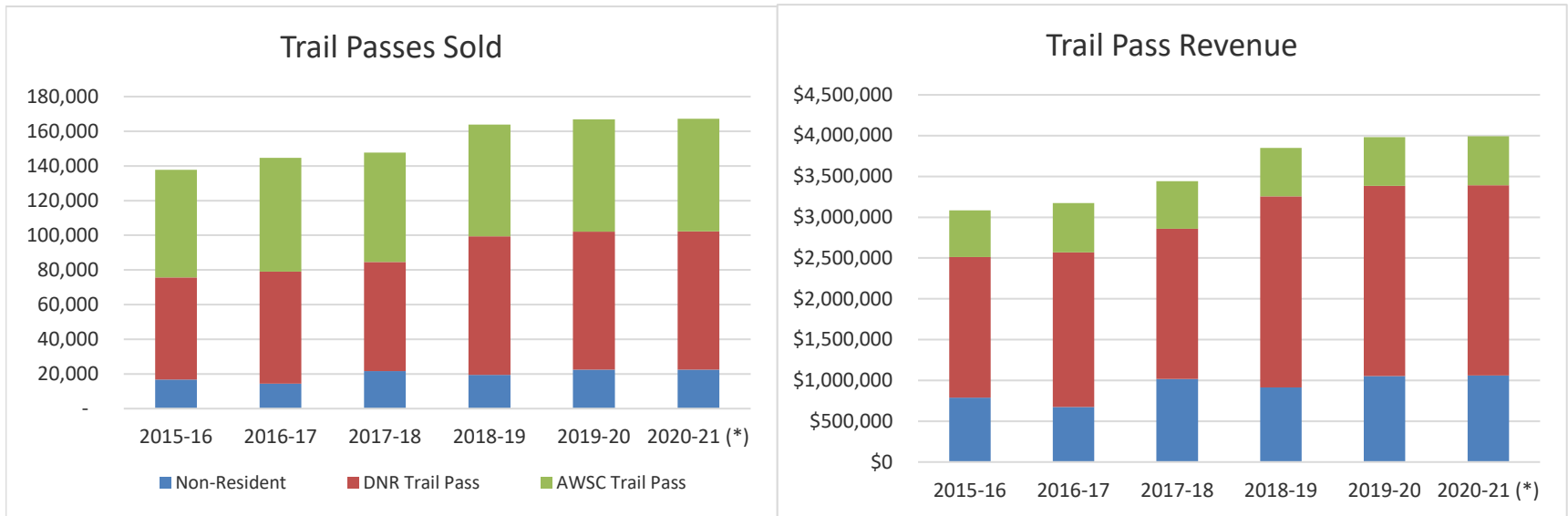
(\*) as of 06/14/21

Registrations expire June 30th annually. Numbers for 20-21 represent partial year summary



Season	Total Snowmobile Trail Passes Sold						Total
	Non-Resident	Revenue (approximate)	DNR Trail Pass	Revenue (approximate)	AWSC Trail Pass	Revenue (approximate)	
2015-16	16,766	\$788,002	58,970	\$1,724,873	61,979	\$573,306	\$3,086,180
2016-17	14,349	\$674,403	64,743	\$1,893,733	65,581	\$606,624	\$3,174,760
2017-18	21,634	\$1,016,798	62,988	\$1,842,399	63,024	\$582,972	\$3,442,169
2018-19	19,477	\$915,419	79,991	\$2,339,737	64,366	\$595,386	\$3,850,541
2019-20	22,438	\$1,054,586	79,619	\$2,328,856	64,757	\$599,002	\$3,982,444
2020-21 (*)	22,532	\$1,059,004	79,780	\$2,333,565	64,906	\$600,381	\$3,992,950

(\*) 2020-21 Totals as of June 14, 2021. Non-Resident and DNR Trail Pass may include some duplicate entries that are rolled back after June 30.



**2020-21 Supplemental Maintenance**

DNR REG	Project	Miles	County/State	Max Amt x\$900	Total Claim Requested =Reg + Supp	Contract Maint.	Supp. Per Request
NO-S	5503	204.2	Ashland	\$183,780	\$77,276.04	\$61,260	16,016.04
NO-S	5505	461.1	Bayfield	\$414,990	\$195,222.88	\$138,330	56,892.88
SCR	5513	272.8	Columbia	\$245,520	\$106,006.07	\$81,840	24,166.07
SCR	5514	398.0	Dane	\$358,200	\$162,927.83	\$119,400	43,527.83
SCR	5515	330.9	Dodge	\$297,810	\$168,863.38	\$99,270	69,593.38
NO-S	5507	318.7	Douglas	\$286,830	\$109,830.46	\$95,610	14,220.46
NO-R	5494	160.8	Florence	\$144,720	\$49,643.17	\$48,240	1,403.17
NER	5463	302.2	Fond du Lac	\$271,980	\$142,827.39	\$90,660	52,167.39
NO-R	5495	406.6	Forest	\$365,940	\$239,561.15	\$121,980	117,581.15
SCR	5517	98.0	Green	\$88,200	\$40,203.20	\$29,400	10,803.20
NER	5464	139.0	Green Lake	\$125,100	\$53,883.30	\$41,700	12,183.30
NO-R	5496	302.6	Iron	\$272,340	\$146,272.98	\$90,780	55,492.98
WCR	5484	142.6	La Crosse	\$128,340	\$55,581.58	\$42,780	12,801.58
NO-R	5497	532.7	Langlade	\$479,430	\$246,229.21	\$159,810	86,419.21
NER	5466	224.7	Manitowoc	\$202,230	\$93,819.15	\$67,410	26,409.15
WCR	5485	791.0	Marathon	\$711,900	\$334,105.03	\$237,300	96,805.03
WCR	5486	308.7	Monroe	\$277,830	\$169,669.00	\$92,610	77,059.00
NER	5469	439.2	Oconto	\$395,280	\$194,950.06	\$131,760	63,190.06
NO-R	5499	429.4	Oneida	\$386,460	\$279,564.82	\$128,820	150,744.82
WCR	5488	226.9	Pierce	\$204,210	\$103,828.87	\$68,070	35,758.87
NO-S	5508	380.9	Polk	\$342,810	\$134,492.10	\$114,270	20,222.10
NO-R	5500	473.0	Price	\$425,700	\$274,226.68	\$141,900	132,326.68
SER	5528	166.5	Racine	\$149,850	\$80,499.66	\$49,950	30,549.66
SCR	5523	215.5	Sauk	\$193,950	\$94,526.55	\$64,650	29,876.55
NO-S	5510	383.2	Sawyer	\$344,880	\$152,869.58	\$114,960	37,909.58
NER	5471	437.8	Shawano	\$394,020	\$145,918.61	\$131,340	14,578.61
SER	5529	236.2	Sheboygan	\$212,580	\$107,822.66	\$70,860	36,962.66
NO-R	5502	494.7	Vilas	\$445,230	\$420,618.60	\$148,410	272,208.60
SER	5530	217.7	Walworth	\$195,930	\$91,113.65	\$65,310	25,803.65
SER	5532	100.9	Waukesha	\$90,810	\$55,581.39	\$30,270	25,311.39
NER	5472	390.6	Waupaca	\$351,540	\$161,758.12	\$117,180	44,578.12
		9,987.1	<b>TOTAL</b>	<b>\$8,988,390</b>	<b>\$4,689,693</b>	<b>\$2,996,130</b>	<b>\$1,693,563</b>

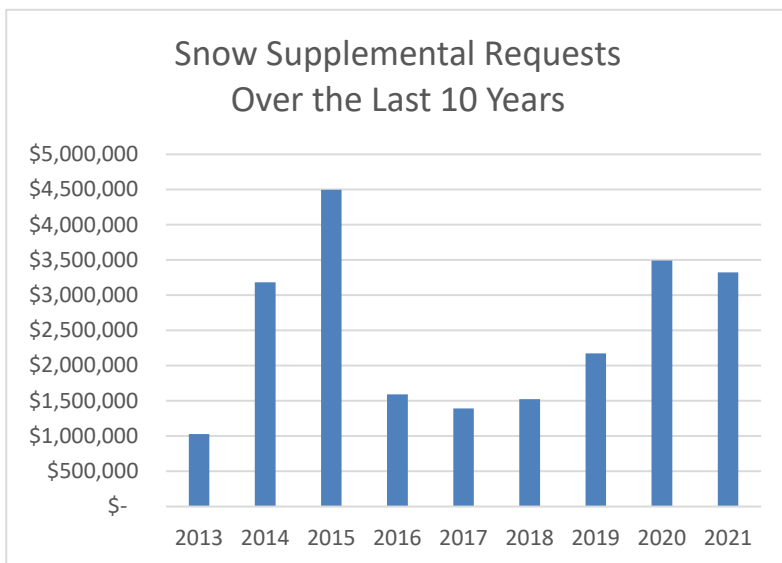
<b>Grand Total</b>	<b>\$4,689,693</b>	<b>\$1,693,563</b>
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## Snowmobile Supplemental Maintenance 10 Year Seasonal History

Snowmobile Season	Fiscal Year	Total Request	Total Payment	Percent of Request Paid	Basic Maint @ \$300/Mi	Per Mile Maint w/ Supp (Statewide Average) *
2011-12**	2013	\$ 1,028,800	\$ 1,028,800	100%	\$ 5,601,780	\$ 355 per mile
2012-13	2014	\$ 3,185,500	\$ 2,515,600	79.0%	\$ 5,601,780	\$ 435 per mile
2013-14***	2015	\$ 4,498,600	\$ 3,016,500	67.2%	\$ 5,658,033	\$ 460 per mile
2014-15	2016	\$ 1,591,493	\$ 1,563,000	100.0%	\$ 5,656,725	\$ 383 per mile
2015-16	2017	\$ 1,393,585	\$ 1,337,771	100.0%	\$ 5,641,215	\$ 371 per mile
2016-17	2018	\$ 1,523,565	\$ 1,255,154	100.0%	\$ 5,684,592	\$ 366 per mile
2017-18	2019	\$ 2,174,711	\$ 2,145,475	100.0%	\$ 5,672,850	\$ 413 per mile
2018-19	2020	\$ 3,492,658	\$ 3,438,074	100.0%	\$ 5,676,180	\$ 482 per mile
2019-20	2021	\$ 3,322,696	\$ 3,303,768	100.0%	\$ 5,676,180	\$ 474 per mile
2020-21	2022	\$ 1,693,563		100.0%	\$ 5,676,180	\$ 381 per mile
10 Year Average		\$ 2,390,517	\$ 2,178,238			\$ 412 per mile

\*\*\*Includes \$301,200 from snowmobile registration revenues.

\* Statewide Average includes all base maintenance payments + supplemental across the state, divided by the total number of funded miles in the state



**10 Year Average = \$2,565,000 / year**

**WISCONSIN SNOWMOBILE PROGRAM  
PRELIMINARY FISCAL YEAR END REPORT JUNE 30, 2021 (FY2021)**

**REGISTRATION REVENUES & EXPENDITURES**

<u>Continuing Balance</u>		
June 30, 2020:		
1	Unobligated Balance	\$7,964,287
2	Encumbered-Unpaid Grant Obligations	\$3,701,658
3	Adjustment from FY 2019 preliminary to FY 2019 Actuals	(\$472,110)
<b>Total Continuing Balance into FY2021</b>		<b>\$11,193,835</b>

<u>FY 2021 Revenues<sup>2</sup></u>		
4	Snowmobile Public Registration <sup>3</sup>	\$1,691,631.00
5	Snowmobile Dealer Registrations	\$4,050.00
6	Snowmobile Municipal Registration	\$110.00
7	Snowmobile Dealer Decal	\$6,840.00
8	Snowmobile Dealer Replacement Decal	\$165.00
9	Snowmobile Replacement Decal	\$12,170.00
10	Snowmobile Replacement Certificate	\$8,930.00
11	Antique Registrations	\$28,355.00
12	Antique Renewals	\$6,510.00
13	Snowmobile Resident Pass	\$2,446,066.50
14	Non-Resident Stickers	\$1,122,468.50
15	Snowmobile Club Pass	\$649,060.00
16	Ownership Transfer Fees	\$128,140.00
17	Interest Distribution	\$130,364.24
18	Refund of Prior Year Expenditures	\$12,595.68
19	Snowmobile Pending	\$862.75
20	Transaction Fees <sup>1</sup>	(\$34,253.53)
21	Agency 855 Revenue	\$308.08
22	NR Trail Pass transfer to Supplement	(\$1,054,586.00)
TOTAL REVENUES COLLECTED		<u>\$5,159,787</u>
<b>TOTAL FY2021 CONTINUING BALANCE AND REVENUES</b>		<b><u>\$16,353,622</u></b>

<u>FY 2021 Expenditures<sup>2</sup></u>		
23	County Trail Maintenance	\$2,135,168
24	County Projects	\$1,348,896
25	County Supplemental	\$0
26	State Trail Maintenance	\$159,030
27	State Projects	\$52,070
28	Snowmobile Enforcement Operations	\$173,240
29	Snowmobile Enforcement/Training <sup>5</sup>	\$119,600
30	County Law Enforcement Aids	\$396,000
31	Aids Admin-Snow Admin	\$206,800
32	Aids Admin-Council	\$2,746
33	Registration Admin	\$528,614
34	Administrative Services	\$40,189
35	DOA Charges	\$308
TOTAL EXPENDITURES		<u>\$5,162,662</u>
36	Encumbered-Unpaid Grant Obligations	\$5,011,394
37	Adjustment - DOA Prior Year Adjustment	\$0
TOTAL OBLIGATIONS		<u>\$5,011,394</u>
<b>TOTAL EXPENDITURES AND OBLIGATIONS</b>		<b><u>\$10,174,055</u></b>
<b>38 PRELIMINARY BALANCE AS OF JUNE 30, 2021</b>		<b><u>\$6,179,567</u></b>
PENDING GRANTS APPROVED BY COUNCIL		\$0
<b>***PRELIMINARY ADJUSTED PROGRAM BALANCE</b>		<b><u>\$6,179,567</u></b>

**ASSIGNED SEGREGATED REVENUE & EXPENDITURES**

<u>Continuing Balance</u>		
June 30, 2020:		
39	Unobligated Balance	\$1,701,275
40	Encumbered-Unpaid Grant Obligations	\$2,167,469
41	Adjustment from FY 2019 preliminary to FY 2019 Actuals	\$18,315
<b>Total Continuing Balance into FY2021</b>		<b>\$3,887,059</b>

<u>FY 2021 Revenues<sup>2</sup></u>		
42	Gas Tax Formula	\$3,306,362
43	Gas Tax Supplement (.55 Multiplier)	\$1,818,499
44	Snowmobile Safety Course Fees	\$33,798
45	NR Trail Pass transfer to Supplement	\$1,054,586
TOTAL REVENUES COLLECTED		<u>\$6,213,245</u>
<b>TOTAL FY 2021 CONTINUING BALANCE AND REVENUES</b>		<b><u>\$10,100,304</u></b>

<u>FY 2021 Expenditures<sup>2</sup></u>		
46	County Trail Maintenance - Gas Tax	\$3,360,848
47	County Project - Gas Tax	(\$974)
48	County Supplemental - Gas Tax	\$1,818,499
49	County Supplemental - Trail Passes	\$1,280,996
50	Law Enforcement and Safety	\$17,997
TOTAL EXPENDITURES		<u>\$6,477,366</u>
51	Encumbered-Unpaid Grant Obligations	\$ 2,024,775
52	Adjustment	\$0
TOTAL OBLIGATIONS		<u>\$2,024,775</u>
<b>TOTAL EXPENDITURES AND OBLIGATIONS</b>		<b><u>\$8,502,141</u></b>
<b>53 PRELIMINARY BALANCE AS OF JUNE 30, 2021</b>		<b><u>\$1,598,163</u></b>
PENDING GRANTS APPROVED BY COUNCIL		
<b>***PRELIMINARY ADJUSTED PROGRAM BALANCE</b>		<b>\$1,598,163</b>
<b>PRELIMINARY ADJUSTED REGISTRATION &amp; ASSIGNED PROGRAM BALANCE</b>		<b>\$7,777,729</b>

March 31, 2020 Registrations (Gas Tax FY21 Revenue)	214,004
Active Vehicles as of June 30, 2021	240,681
Vehicles registered during FY21	57,679
WI Resident Trail Passes Sold during FY21	81,662
Discount Trail Passes Sold during FY21	687
NR Trail Passes Sold during FY21	22,523

**Footnotes**

<sup>1</sup> These are contra revenues NOT expenses. They are classified on DNR's accounting system as contra revenues and our revenue total will not tie out without these in this section. A contra revenue is defined a deduction from the gross revenue reported by a business, which results in net revenue.

<sup>2</sup> The question has been raised why aren't expenses shown as a negative on this financial. We did this for simplicity. True accounting format would have revenues with a negative balance, and contra revenues and expenses with a positive balance. This can be confusing to users so we chose to present all amounts as positive.

<sup>3</sup> In FY's 2016 and prior, registrations were presented as new and original. This will change with the FY 17 financial statement. Go Wild now only tracks whether the registration is public or private so that is how this information will be shown going forward.

<sup>4</sup> Commercial Owner Licenses and Commercial Owner Plates will now be broken down into 3 categories - Dealer Decals, Dealer Registration, Dealer Replacement Decal.

<sup>5</sup> Snowmobile Enforcement and Training Costs are moved onto this appn (370) if necessary. There are other appns within DNR that can pay for these costs. Those other appns do not appear on this financial statement. In a low snow year, we may not see any charges here. That is not because there were no snowmobile enforcement and training costs, but rather a different appn covered those costs. The decision of how much cost to transfer here is made by the Law Enforcement Management Accountant.

Reference #	Description of Line Item
<b>Revenues (unassigned)</b>	
1	Prior year unused balance from Preliminary Fiscal Year End Report June 30, 2019 (FY2019)
2	Carryover encumbrances from FY2019
3	The report from FY2019 used preliminary information. This line adjusts the preliminary information to the actual balance per the final condition statement.
4	#C x \$30 less expenses (C = Active registrations during FY)
5	\$30 x dealer registrations less transaction expenses
6	Municipal registrations
7	\$30 x dealer registrations less transaction expenses
8	Snomobile Dealer Replacement Decals
9	Snowmobile Replacement Decals
10	Snowmobile Replacement Certificates
11	Antique Registrations
12	Antique Renewals
13	# x \$30 less transaction expense
14	# x \$50 less transaction expense
15	# X \$10 less transaction expense
16	\$5 per less transaction expense
17	Interest distribution for snowmobile accounts
18	Grant from a prior year that were either cancelled or cost less than the advance, so money was returned to the snowmobile fund.
19	Pending financial transactions
20	Finance/accounting item for licensing agents
21	Revenue from other sources.
22	Moves to segregated side because must be used for supplemental maintenance first.
<b>Expenses</b>	
23	Appropriation 574 Maintenance
24	Appropriation 574 Development
25	Appropriation 574 supplemental
26	Maintenance on DNR-managed trails
27	Development project expenditures on DNR-managed trails
28	Appropriation 361 law enforcement operations.
29	Snowmobile Enforcement and Training Costs are moved onto this appn (370) if necessary. There are other appns within DNR that can pay for these costs. Those other appns do not appear on this financial statement. In a low snow year, we may not see any charges here. That is not because there were no snowmobile enforcement and training costs, but rather a different appn covered those costs. The decision of how much cost to transfer here is made by the Law Enforcement Management Accountant.
30	Appropriation 552
31	All state administration: Staff from grants, registration, finance, technology services and DNR & DOA administrative charges.
32	Council expenditures during FY (Council's budget)
33	All state administration: Staff from grants, registration, finance, technology services and DNR & DOA administrative charges.
34	All state administration: Staff from grants, registration, finance, technology services and DNR & DOA administrative charges.
35	All state administration: Staff from grants, registration, finance, technology services and DNR & DOA administrative charges.
36	Amount committed to grants that haven't been completed at fiscal year end (maintenance and/or development)
37	For miscellaneous Adjustments
38	Preliminary adjusted program balance
<b>Assigned Revenue (must be used for certain purposes)</b>	
39	We spend gas tax funds first, the carryover hear is due to the NR trail pass supplemental surplus we're maintaining for a big snow year. This balance is close to \$0.00 after a big snow year when supplemental is pro-rated.
40	Carryover encumbrances from FY2019
41	The report from FY2019 used preliminary information. This line adjusts the preliminary information to the actual balance per the final condition statement.
42	Registrations from prior year x 50 x .309 = Gas Tax Transfer amount
43	#41 x .55 = Gas Tax Supplement amount
44	Snowmobile Safety Education Revenue
45	#22 moved to the assigned revenue side for supplemental use
<b>Expenses</b>	
46	Appropriation 575 Maintenance
47	Appropriation 575 Development
48	Appropriation 575 Supplementatl
49	Appropriation 569 Supplemental
50	Safety and Training expenses - can vary based on Law Enforcement accountant's discretion and lapse requirements
51	Amount committed to grants that haven't been completed at fiscal year end (maintenance and/or development) - funds have been reserved on the accounting system
52	Miscellaneous adjustments.
53	Used to determine available funds for development grants (August funding meeting)

**Snowmobile Funding Available 2021-2022 (FY22)**

**General Segregated Funding**

Unallocated opening balance:	\$6,179,566.68
Appr 574 opening balance:	\$2,543,722.09
Appr 574 FY22 spending authority:	\$5,675,400.00
Estimate FY22 general segregated revenue (3-year average):	\$5,686,700.88
Estimated FY22 non-development general expenditures (3-year average):	\$1,564,249.51
Estimated break even general segregated development expenditures:	\$4,122,451.38
<b>Total appr 574 full chapter 20 authority</b>	<b>\$8,219,122.09</b>
<b>Total appr 574 break even</b>	<b>\$6,666,173.47</b>

**Assigned Segregated Funding**

Appr 575 (gas tax) opening balance:	\$11,957.02
FY22 gas tax revenue (214,004 active vehicles):	\$3,306,361.94
FY22 gas tax multiplier (55% of Gas Tax Total):	\$1,818,499.06
<b>Total appr 575</b>	<b>\$5,136,818.02</b>

Appr 569 (nonresident trail pass) opening balance:	\$1,480,409.11
FY22 Nonresident trail pass transfer:	\$1,058,581.00
<b>Total Appr 569</b>	<b>\$2,538,990.11</b>

**Non-development Expenses**

Maintenance - \$300*19,399.35 mi (FY22):	(\$5,819,805.00)
Supplemental requests:	(\$1,657,804.00)
<b>Total Expenses</b>	<b>(\$7,477,609.00)</b>

**Option 1: Spend full chapter 20 authority and assigned funding**

Available development funding:	\$8,417,321.22
Unallocated general segregated funding:	\$4,626,618.06
Appr 575 carry forward:	\$0.00
Appr 569 carry forward:	\$0.00

**Option 2: Maintain FY22-23 maintenance in general segregated balance and keep \$1M nonresident trail pass revenue in reserve**

Available development funding:	\$6,143,939.28
Unallocated general segregated funding:	\$5,900,000.00
Appr 575 carry forward:	\$0.00
Appr 569 carry forward:	\$1,000,000.00

**Option 3: Maintain general segregated balance and keep nonresident trail pass revenue in reserve**

Available development funding:	\$4,325,382.49
Unallocated general segregated funding:	\$6,179,566.68
Appr 575 carry forward:	\$0.00
Appr 569 carry forward:	\$2,538,990.11

**Annual Maintenance**

<b>21-22 Miles</b>	<b>20-21 Miles</b>	<b>County/State</b>	<b>Contract Maintenance</b>	<b>Suppl. Max</b>
<b>Northeast Region</b>				
198.5	198.5	BROWN	\$59,550	\$178,650
143.9	143.9	CALUMET	\$43,170	\$129,510
234.5	234.5	DOOR	\$70,350	\$211,050
303.4	303.4	FOND DU LAC	\$91,020	\$273,060
139.0	139.0	GREEN LAKE	\$41,700	\$125,100
177.9	177.9	KEWAUNEE	\$53,370	\$160,110
224.7	224.7	MANITOWOC	\$67,410	\$202,230
539.0	539.0	MARINETTE	\$161,700	\$485,100
145.6	145.6	MARQUETTE	\$43,680	\$131,040
439.2	439.2	OCONTO	\$131,760	\$395,280
299.3	299.3	OUTAGAMIE	\$89,790	\$269,370
454.4	437.8	SHAWANO	\$136,320	\$408,960
390.6	390.6	WAUPACA	\$117,180	\$351,540
286.1	286.1	WAUSHARA	\$85,830	\$257,490
137.5	137.5	WINNEBAGO	\$41,250	\$123,750
<b>West Central Region</b>				
199.4	199.4	ADAMS	\$59,832	\$179,496
239.1	239.1	BUFFALO	\$71,730	\$215,190
400.3	400.3	CHIPPEWA	\$120,090	\$360,270
376.2	376.2	CLARK	\$112,860	\$338,580
115.9	116.8	CRAWFORD	\$34,782	\$104,346
310.5	310.5	DUNN	\$93,150	\$279,450
181.2	181.2	EAU CLAIRE	\$54,360	\$163,080
312.0	312.0	JACKSON	\$93,600	\$280,800
247.4	247.4	JUNEAU	\$74,220	\$222,660
142.6	142.6	LA CROSSE	\$42,780	\$128,340
770.8	791.0	MARATHON	\$231,231	\$693,693
308.7	308.7	MONROE	\$92,610	\$277,830
119.3	119.3	PEPIN	\$35,790	\$107,370
226.9	226.9	PIERCE	\$68,070	\$204,210
291.1	291.1	PORTAGE	\$87,330	\$261,990
227.8	227.8	ST. CROIX	\$68,340	\$205,020
234.9	234.9	TREMPEALEAU	\$70,470	\$211,410
246.5	246.5	VERNON	\$73,950	\$221,850
265.9	265.9	WOOD	\$79,770	\$239,310
<b>Northern Region - East</b>				
160.8	160.8	FLORENCE	\$48,240	\$144,720
406.6	406.6	FOREST	\$121,980	\$365,940
302.6	309.3	IRON	\$90,786	\$272,358
532.7	532.7	LANGLADE	\$159,810	\$479,430
305.7	293.0	LINCOLN	\$91,710	\$275,130
428.7	429.4	ONEIDA	\$128,610	\$385,830
473.0	473.0	PRICE	\$141,900	\$425,700
341.1	341.1	TAYLOR	\$102,330	\$306,990
494.7	494.7	VILAS	\$148,410	\$445,230



**Annual Maintenance**

<b>21-22 Miles</b>	<b>20-21 Miles</b>	<b>County/State</b>	<b>Contract Maintenance</b>	<b>Suppl. Max</b>
<b>Northern Region - West</b>				
204.2	204.2	ASHLAND	\$61,260	\$183,780
303.3	303.3	BARRON	\$90,990	\$272,970
461.1	461.1	BAYFIELD	\$138,330	\$414,990
250.6	250.6	BURNETT	\$75,180	\$225,540
318.7	318.7	DOUGLAS	\$95,610	\$286,830
380.9	380.9	POLK	\$114,270	\$342,810
323.0	316.1	RUSK	\$96,900	\$290,700
383.2	383.2	SAWYER	\$114,954	\$344,862
244.3	243.3	WASHBURN	\$73,290	\$219,870
61.2	61.2	TRI CO COMM	\$18,360	\$55,080
<b>South Central Region</b>				
272.8	272.8	COLUMBIA	\$81,840	\$245,520
399.6	398.0	DANE	\$119,880	\$359,640
343.5	312.9	DODGE	\$103,050	\$309,150
387.4	387.4	GRANT	\$116,220	\$348,660
98.0	98.0	GREEN	\$29,400	\$88,200
152.3	152.3	IOWA	\$45,690	\$137,070
224.4	199.2	JEFFERSON	\$67,320	\$201,960
104.4	104.4	LAFAYETTE	\$31,320	\$93,960
141.3	141.3	RICHLAND	\$42,390	\$127,170
174.0	174.0	ROCK	\$52,200	\$156,600
215.5	215.5	SAUK	\$64,650	\$193,950
57.5	57.5	CHEESE COUNTRY(Tri-C	\$17,250	\$51,750
<b>South East Region</b>				
77.9	77.9	KENOSHA	\$23,370	\$70,110
9.3	9.3	MILWAUKEE	\$2,790	\$8,370
112.0	112.0	OZAUKEE	\$33,600	\$100,800
166.5	166.5	RACINE	\$49,950	\$149,850
236.2	236.2	SHEBOYGAN	\$70,860	\$212,580
217.7	217.7	WALWORTH	\$65,310	\$195,930
203.3	203.3	WASHINGTON	\$60,990	\$182,970
99.2	99.2	WAUKESHA	\$29,760	\$89,280
<b>19,399.35</b>	<b>19,333.18</b>		<b>\$5,819,805</b>	<b>\$17,459,415.00</b>

### 2021-22 Snowmobile Funding Summary

<u>Category</u>	<u>Funds Available</u>	<u>Amount Requested</u>	<u>Amount Funded</u>	<u>Balance</u>
Funds Available	\$ 11,963,744			\$ 11,963,744
Maintenance	(19,400 @ \$300/mile)	\$ 5,820,000	\$ 5,820,000	\$ 6,143,744
Reserve per FN Committee			\$ 1,000,000	\$ 5,143,744
Groomer Tracking Systems GPS			\$ 200,000	\$ 4,943,744
<u>Funds For Development, etc</u>	\$ -			\$ 4,943,744
Storm Damage		\$ -	\$ -	\$ 4,943,744
Cost Increase Requests		\$ 277,577	\$ 277,578	\$ 4,666,166
Snow Bridge Rehab		\$ 2,203,543	\$ 1,712,294	\$ 2,953,872
Snow Trail Rehab		\$ 215,541	\$ 209,041	\$ 2,744,831
Snow Relocation Mandatory		\$ 27,607	\$ 27,607	\$ 2,717,224
Snow Relocation Discretionary		\$ 735,800	\$ 735,800	\$ 1,981,424
Snow Bridge New		\$ 423,750	\$ -	\$ 1,981,424
New Miles	(607 @ \$300/mile)	\$ 184,258	\$ 145,498	\$ 1,835,926
Snow Acquisition		\$ -	\$ -	\$ 1,835,926
Snow Non-Trail Development		\$ 42,463	\$ -	\$ 1,835,926
<u>Development Total</u>		\$ 4,110,538		\$ 1,835,926
Financial Aids Total		\$ 14,041,076	\$ 10,127,818	
Prelim. Supplemental (20-21):	\$ 1,657,804.00			

## 2021-22 SNOWMOBILE COST INCREASE REQUESTS

Region	County		New/Increase Component	New Costs	Total	Approved	Comments	
<b>#90</b>	Calumet	Calumet County is requesting a cost amendment due to the increase in lumber and steel prices.	<b>Increase Request</b>		\$5,100	\$5,100		
			materials	\$5,100	After Cost Share: <b>\$5,100</b>			
		Other funds received, requested, or committed? Yes or No: <input type="text" value="no"/> Amount: <input type="text"/>						
		<b>Original Grant #</b> S-5543	<b>Funding Cat.</b> bridge rehab					
		<b>Previous Grant Amt</b> \$32,600	<b>New Grant Amt (After)</b> \$37,700			\$5,100		
<b>#91</b>	Green Lake	Green Lake County is requesting a cost amendment due to the increase in materials cost.	<b>Increase Request</b>		\$11,529	\$11,529		
			materials	\$11,529	After Cost Share: <b>\$11,529</b>			
		Other funds received, requested, or committed? Yes or No: <input type="text" value="No"/> Amount: <input type="text"/>						
		<b>Original Grant #</b> S-5540	<b>Funding Cat.</b> bridge rehab					
		<b>Previous Grant Amt</b> \$28,058	<b>New Grant Amt (After)</b> \$39,587			\$11,529		
<b>#92</b>	Polk	AMENDMENT REQUEST. Polk Co is requesting an amendment to grant S-5585 Lorain Snowmobile Bridge due to the 35 percent increase in wooden timber costs. Abutments and pillars need to be added to the bridge approaches to secure the bridge. Total cost will now be \$53,992.48	<b>Increase Request</b>		\$26,123	\$26,123	Why was this not anticipated? Does county have a permit?	
			Materials	\$26,123	After Cost Share: <b>\$26,123</b>			
		Other funds received, requested, or committed? Yes or No: <input type="text" value="No"/> Amount: <input type="text"/>						
		<b>Original Grant #</b> S-5585	<b>Funding Cat.</b> bridge rehab					
		<b>Previous Grant Amt</b> \$27,869	<b>New Grant Amt (After)</b> \$53,992			\$26,123		

## 2021-22 SNOWMOBILE COST INCREASE REQUESTS

Region	County		New/Increase Component	New Costs	Total	Approved	Comments
<b>#93</b>	NOR-R Langlade	AMENDMENT REQUEST. Langlade County is requesting additional funds to cover the increased cost of materials and labor on the Neva Knight Culvert Replacement project. The cost of the culvert, labor and other materials have increased since the project's approval last year. The project is split between ATV, Snowmobile and RTP.	<b>Increase Request</b>		\$3,205	\$1,602	
			Materials & Lab	\$3,205	After Cost Share: <b>\$1,602</b>		
		Other funds received, requested, or committed? Yes or No: <input type="checkbox"/> Yes <input type="checkbox"/> No      Amount: <b>\$ 1,603</b>					
		<b>Original Grant # S-5546</b>	<b>Funding Cat. bridge rehab</b>				
		<b>Previous Grant Amt \$10,849</b>	<b>New Grant Amt (After) \$12,451</b>		\$1,602		
<b>#94</b>	NOR-R Price	AMENDMENT REQUEST. Price County is requesting additional funds to cover the change in project costs due to a highway realignment. This has changed the length and location of the bridge to be installed. The county is requesting additional funds to complete the project. This project uses only snowmobile funds.	<b>Increase Request</b>		\$169,000	\$169,000	Need 2nd estimate. Why 14k, consider more substantial bridge? Is the timeframe in conjunction with the road project? - - See attachment additional info and updated cost estimates
			Materials & Lab	\$169,000	After Cost Share: <b>\$169,000</b>		
		Other funds received, requested, or committed? Yes or No: <input type="checkbox"/> no <input type="checkbox"/> Yes <input type="checkbox"/> No      Amount: <b>\$</b>					
		<b>Original Grant # S-5306</b>	<b>Funding Cat. bridge rehab</b>				
		<b>Previous Grant Amt \$35,000</b>	<b>New Grant Amt (After) \$204,000</b>		\$169,000		
<b>#95</b>	WCR-N Chippewa	AMENDMENT REQUEST: Chippewa County is requesting additional funds to rehab a 12' x 40' bridge on Corridor 27. The original proposal (last year) was rehab only to stabilize the upstream banks and protect pilings. It has since been determined that this will be insufficient, and taht the bridge has further deteriorated and requires replaced with a 12' x 50' bridge..	<b>Increase Request</b>		\$62,568	\$62,568	
			Bridge Structure	\$24,000	After Cost Share: <b>\$62,568</b>		
			Engineering	\$2,000			
			Site Prep	\$2,000			
			Pilings	\$6,000			
	Approaches	\$2,000					
	Riprap	2000					
	Install Labor	\$30,000					
	Trail Rehab	\$7,350					
	Existing Grant	-\$12,782					
	Other funds received, requested, or committed? Yes or No: <input type="checkbox"/> <input type="checkbox"/> Amount: <b>\$</b>						
<b>Original Grant # S-5413</b>	<b>Funding Cat. bridge rehab</b>						
<b>Previous Grant Amt \$12,782</b>	<b>New Grant Amt (After) \$75,350</b>		\$62,568				

## 2021-22 SNOWMOBILE COST INCREASE REQUESTS

Region	County		New/Increase Component	New Costs	Total	Approved	Comments
#96	Forest	AMENDMENT REQUEST. Forest County is requesting additional funds to cover the increased cost of materials and labor on the Nicolet Trail Camp 8 Bridge project. All work is now complete. However, there was a cost overrun of \$3,310.53. The project is split between ATV and Snowmobile.	<b>Increase Request</b>		\$3,311		\$1,656
			Materials & Lab	\$3,311	After Cost Share: <b>\$1,655</b>		
			Other funds received, requested, or committed?				
			Yes or No: <input type="checkbox"/> Yes <input type="checkbox"/> No	Amount: <b>\$ 1,656</b>			
			<b>Original Grant #</b> S-5314	<b>Funding Cat.</b> bridge rehab			
<b>Previous Grant Amt</b> \$211,050	<b>New Grant Amt (After)</b> \$212,705			\$1,655			
			<b>TOTAL</b>		<b>\$277,577</b>	<b>\$277,578</b>	

## 2021-22 SNOWMOBILE BRIDGE REHABILITATION REQUESTS

Region	County		Component	Costs	Total	Approved	Q & A
<b>#1</b>	Iron	Iron County proposes to replace the wear surface, deck, and railings on the Trail 2 bridge over Vaughn Creek, near Townline Rd. The trail is also open to ATVs and UTVs. The project is proposed for 50/50 split with ATV/UTV grant funds.	*Structure		\$14,940	\$7,470	
		Engineer			After Cost Share:		
		Permits			<b>\$7,470</b>		
		Site prep		\$9,160			
		<b>Other funds received, requested, or committed?</b>					
		<b>Yes or No:</b> Y	<b>Amount:</b> 7470				
Variance or Exception?		Public or Private: Public	Bridge Inventory #: <b>Iron Co 34</b>				
		Old Bridge Size: 40 ft	New Bridge Size: N/A				
		Water Body: Vaughn Creek	Labor Source: Force				
		Years in System: 20+	Design Weight Load: 25,000				
			# of New Mi	0	\$7,470		
<b>#2</b>	Iron	Iron County proposes to replace the wear surface, deck, and railings on the Trail 8 bridge over Layman's Creek. The trail is only used for snowmobiles. <b>Application withdrawn, Landowner is purchasing materials.</b>	*Structure		\$0	\$0	-- landowner agreement and waiver? <b>Application withdrawn, landowner is purchasing materials.</b>
		Engineer			After Cost Share:		
		Permits			<b>\$0</b>		
		Site prep					
		<b>Other funds received, requested, or committed?</b>					
		<b>Yes or No:</b> No	<b>Amount:</b>				
Variance or Exception?		Public or Private: Private	Bridge Inventory #: <b>Iron Co 28</b>				
		Old Bridge Size: 60 ft	New Culvert Size: N/A				
		Water Body: Layman's Creek	Labor Source: Force				
		Years in System: 10+	Design Weight Load: 25,000				
			# of New Mi	0	\$0		
<b>#3</b>	Iron	Iron County proposes to replace the existing bridge over Cominski Creek on Trail 2a with a new 12'x40' bridge. The current bridge consists of timber mats and is considerably undersized and in poor shape. The trail is only used for snowmobiles.	*Structure	\$16,140	\$23,900	\$23,900	-- landowner agreement and waiver? <b>Yes, rcvd</b>
		Engineer			After Cost Share:		
		Permits			<b>\$23,900</b>		
		Site prep	\$1,000				
		<b>Other funds received, requested, or committed?</b>					
		<b>Yes or No:</b> No	<b>Amount:</b>				
Variance or Exception?		Public or Private: Private	Bridge Inventory #: <b>Iron Co 37</b>				
		Old Bridge Size: 23' x 14'	New Bridge Size: 40' x 12'				
		Water Body: Cominski Creek	Labor Source: Force				
		Years in System: 10+	Design Weight Load: 25,000				
			# of New Mi	0	\$23,900		

## 2021-22 SNOWMOBILE BRIDGE REHABILITATION REQUESTS

Region	County		Component	Costs	Total	Approved	Q & A
#4	Forest	Forest County proposes to replace the existing Mill Pond Bridge on the Nicolet State Trail with a new 12'x85' bridge. The current bridge is over 60 years old and in very poor condition. The county has been tentatively awarded RTP funds and is also applying for ATV grant funds for this project.	*Structure	\$121,200	After Cost Share: <b>\$71,556</b>	\$71,556	If RTP funds awarded, will they be able to honor the Buy America requirement? -- Forest County indicates they are confident they can comply with Buy America requirements for RTP.
			Engineer	\$33,000			
			Permits	\$19,750			
			Site prep	\$5,500			
			Materials				
Abutments	\$7,500						
			Pilings/Piers				
			Approaches	\$2,000			
			Culverts				
			Riprap	\$1,200			
			Labor	\$18,500			
			Equip Rental	\$25,500			
			Remove/disp	\$8,000			
			# of New Mi	0	\$71,556		
			<b>Other funds received, requested, or committed?</b>				
			<b>Yes or No:</b>	<b>Yes</b>	<b>Amount:</b>	<b>170594</b>	
			Public or Private:	Public	Bridge Inventory #:	<b>DNR 21-014</b>	
			Old Bridge Size:	12' x 85'	New Bridge Size:	12' x 85'	
			Water Body:	Mill Pond	Labor Source:	Contract	
			Years in System:	20+	Design Weight Load:	25,000	
			Variance or Exception?				
#5	Forest	Forest County proposes to replace the existing Little Popple River North Bridge on the Nicolet State Trail with a new 12'x90' bridge. The current bridge is over 60 years old and in poor condition. The county has been tentatively awarded RTP funds and is also applying for ATV grant funds for this project.	*Structure	\$121,200	After Cost Share: <b>\$73,806</b>	\$73,806	If RTP funds awarded, will they be able to honor the Buy America requirement? -- Forest County indicates they are confident they can comply with Buy America requirements for RTP.
			Engineer	\$33,000			
			Permits	\$19,750			
			Site prep	\$6,000			
			Materials				
Abutments	\$8,500						
			Pilings/Piers				
			Approaches	\$2,000			
			Culverts				
			Riprap	\$1,200			
			Labor	\$20,000			
			Equip Rental	\$27,000			
			Remove/disp	\$8,000			
			# of New Mi	0	\$73,806		
			<b>Other funds received, requested, or committed?</b>				
			<b>Yes or No:</b>	<b>Yes</b>	<b>Amount:</b>	<b>172844</b>	
			Public or Private:	Public	Bridge Inventory #:	<b>DNR 21-011</b>	
			Old Bridge Size:	12' x 90'	New Bridge Size:	12' x 90'	
			Water Body:	Little Popple Rvr	Labor Source:	Contract	
			Years in System:	20+	Design Weight Load:	25,000	
			Variance or Exception?				
#6	Forest	Forest County proposes to replace the existing Little Popple River Center Bridge on the Nicolet State Trail with a new 12'x90' bridge. The current bridge is over 60 years old and in poor condition. The county has been tentatively awarded RTP funds and is also applying for ATV grant funds.	*Structure	\$103,100	After Cost Share: <b>\$59,756</b>	\$59,756	Better pictures of sill plates, etc would show the need. What's wrong with the superstructure? - - Engineering report with additional pictures included in application.
			Engineer	\$33,000			
			Permits	\$19,750			
			Site prep	\$5,000			
			Materials				
Abutments	\$7,000						
			Pilings/Piers				
			Approaches	\$2,000			
			Culverts				
			Riprap	\$1,200			
			Labor	\$18,000			
			Equip Rental	\$22,500			
			Remove/disp	\$7,000			
			# of New Mi	0	\$59,756		
			<b>Other funds received, requested, or committed?</b>				
			<b>Yes or No:</b>	<b>Yes</b>	<b>Amount:</b>	<b>158794</b>	
			Public or Private:	Public	Bridge Inventory #:	<b>DNR 21-010</b>	
			Old Bridge Size:	12' x 80'	New Bridge Size:	12' x 80'	
			Water Body:	Little Popple Rvr	Labor Source:	Contract	
			Years in System:	20+	Design Weight Load:	25,000	
			Variance or Exception?				

## 2021-22 SNOWMOBILE BRIDGE REHABILITATION REQUESTS

Region	County		Component	Costs	Total	Approved	Q & A
<b>#7</b>	Vilas	Vilas County proposes to replace the existing bridge over Verna Creek on Corridor Trail 10 with a new 12'x30' bridge. The current bridge is over 30 years old and not in good condition. The trail is only used for snowmobiles.	*Structure	\$38,100	\$160,350 After Cost Share: <b>\$160,350</b>	\$0	<i>Why so expensive? Price seems high for a 30' bridge. Why is labor so high? Project Management? -- County has provided another bridge estimate for consideration. This is included in the application.</i>
		Engineer	\$27,000				
		Permits	\$1,000				
		Site prep	\$1,000				
Materials							
Abutments	\$22,250						
Pilings/Piers	\$1,000						
Approaches	\$10,000						
Culverts							
Riprap							
Labor	\$60,000						
Equip Rental							
Other							
<b>Other funds received, requested, or committed?</b>							
<b>Yes or No:</b>		No	<b>Amount:</b>				
Public or Private:		Public	Bridge Inventory #:		<b>Vilas 4</b>		
Variance or Exception?		Old Bridge Size: 12' x 30'	New Bridge Size: 12' x 30'				
		Water Body: Verna Creek	Labor Source: Contract				
		Years in System: 30+	Design Weight Load: 25,000				
			# of New Mi	0	\$160,350		
<b>#8</b>	Door	Door County proposes to replace the Krueger Creek Bridge, on Corridor 3, with a 26' X 12' bridge. Temporary repairs were made for the 20-21 season after a partial failure, but a replacement is needed.	*Structure	\$25,000	\$37,200 After Cost Share: <b>\$37,200</b>	\$37,200	<i>Routinely maintained? Yes, log jam north of the bridge let loose during a storm and caused damage to the bridge, banks, and underside of the bridge.</i>
		Engineer	\$4,000				
		Permits					
		Site prep	\$900				
Materials							
Abutments							
Pilings/Piers							
Approaches							
Culverts							
Riprap							
Labor							
Equip Rental	\$5,500						
Other	\$1,800						
<b>Other funds received, requested, or committed?</b>							
<b>Yes or No:</b>		No	<b>Amount:</b>				
Public or Private:		Private	Bridge Inventory #:		<b>Door-2</b>		
Variance or Exception?		Old Bridge Size: 26' X 12'	New BridgeSize: 26' X 12'				
		Water Body: Krueger Creek	Labor Source: contractor				
		Years in System: 33	Design Weight Load: 25,000				
			# of New Mi	0	\$37,200		
<b>#9</b>	Shawano	Shawano County proposes to replace decking and railings on the Corridor 9 North bridge over the Red River. Split with ATV.	*Structure		\$9,745 After Cost Share: <b>\$4,873</b>	\$4,873	<i>Who will do the work? Club. Plywood used to cover deck for protection. Replaced 3 times.</i>
		Engineer					
		Permits					
		Site prep					
Materials	\$9,745						
Abutments							
Pilings/Piers							
Approaches							
Culverts							
Riprap							
Labor							
Equip Rental							
Other							
<b>Other funds received, requested, or committed?</b>							
<b>Yes or No:</b>		Yes	<b>Amount:</b>		<b>\$ 4,872</b>		
Public or Private:		Private	Bridge Inventory #:				
Variance or Exception?		Old Bridge Size: 12' X 48'	New Culvert Size: same				
		Water Body: Red River	Labor Source: club				
		Years in System: 21	Design Weight Load: 30K				
			# of New Mi	0	\$4,873		



## 2021-22 SNOWMOBILE BRIDGE REHABILITATION REQUESTS

Region	County		Component	Costs	Total	Approved	Q & A
<b>#10</b>	Barron	Barron County proposes to replace an existing bridge over Lower Pine Creek known as Corridor 37 Bridge. The bridge has significant deterioration and structural loss. Inspection determined unsafe and bridge was closed.	*Structure	\$16,900	\$51,675 After Cost Share: <b>\$51,675</b>	\$51,675	When was this bridge last rehabed with grant funds? <i>No grant funds used</i> Length of trail use agreement? Deeded?
			Engineer	\$1,000			
			Permits				
			Site prep	\$2,175			
			Materials				
			Abutments	\$4,500			
			Pilings/Piers				
			Approaches	\$4,500			
			Culverts				
			Riprap	\$2,000			
			Labor	\$18,100			
			Equip Rental				
			Other	\$2,500			
			# of New Mi	0	\$51,675		
			<b>Other funds received, requested, or committed?</b>				
			<b>Yes or No:</b>	N	<b>Amount:</b>		
			Public or Private:	Private	Bridge Inventory #:	12413	
Variance or Exception?			Old Bridge Size:	42' x 14.5'	New Bridge Size:	50' x 12'	
			Water Body:	Lwr Pine Crk	Labor Source:		
			Years in System:	32	Design Weight Load:	30,000	
<b>#11</b>	Barron	Barron County proposes to rehabilitate a bridge across Pokegama Creek on Corridor 12. The bridge deck has failed and needs replacing.	*Structure		\$21,050 After Cost Share: <b>\$21,050</b>	\$21,050	
			Engineer				
			Permits				
			Site prep				
			Materials	\$13,550			
			Abutments				
			Pilings/Piers				
			Approaches				
			Culverts				
			Riprap				
			Labor	\$7,500			
			Equip Rental				
			Other				
			# of New Mi	0	\$21,050		
			<b>Other funds received, requested, or committed?</b>				
			<b>Yes or No:</b>	N	<b>Amount:</b>		
Variance or Exception?			Public or Private:	Private	Bridge Inventory #:	12399	
			Old Bridge Size:		New Bridge Size:		
			Water Body:	Pokegama	Labor Source:		
			Years in System:	38	Design Weight Load:	30,000	
<b>#12</b>	Rusk	Rusk Co proposes to replace an aging snowmobile bridge over Potato Creek known as Trail 131 bridge. The bridge is too short for the site and is positioned too low to provide the proper clearance.	*Structure	\$54,000	\$80,000 After Cost Share: <b>\$80,000</b>	\$80,000	
			Engineer	\$3,000			
			Permits				
			Site prep	\$1,000			
			Materials				
			Abutments				
			Pilings/Piers	\$12,000			
			Approaches				
			Culverts				
			Riprap				
			Labor	\$2,000			
			Equip Rental	\$5,000			
			Other	\$3,000			
			# of New Mi	0	\$80,000		
			<b>Other funds received, requested, or committed?</b>				
			<b>Yes or No:</b>	No	<b>Amount:</b>		
Variance or Exception?			Public or Private:	Public	Bridge Inventory #:	31	
			Old Bridge Size:	12' x 52'	New Bridge Size:	12' x 60'	
			Water Body:	Potatoe Crk	Labor Source:		
			Years in System:	24	Design Weight Load:	25,000	

## 2021-22 SNOWMOBILE BRIDGE REHABILITATION REQUESTS

Region	County		Component	Costs	Total	Approved	Q & A
<b>#13</b>	NOR-S Tri Co Rec	Tri- County Recreation Trail Comm proposes to replace the culvert for Miller Creek which has failed. Project would include removing and replacing the culvert and building the trail back up to the original 6" gravel base. Total Project \$76,075.00 Snow \$38,037.52 ATV \$38,037.52	*Structure	\$35,366	\$76,075 After Cost Share: <b>\$38,038</b>	\$38,038	
			Engineer				
			Permits				
			Site prep				
			Materials	\$10,924			
			Abutments				
			Pilings/Piers				
			Approaches				
			Culverts				
			Riprap				
			Labor	\$29,785			
			Equip Rental				
			Other				
			# of New Mi	0	\$38,038		
		<b>Other funds received, requested, or committed?</b>					
		<b>Yes or No:</b>	Yes	<b>Amount:</b>	<b>\$ 38,037.50</b>		
		Public or Private:	Public	Bridge Inventory #:			
		Variance or Exception?	Old Bridge Size:	New Bridge Size:	9' x 110'		
			Water Body:	Labor Source:			
			Miller Creek	Design Weight Load:			
			Years in System:				
<b>#14</b>	WCR-N Chippewa	Chippewa Co proposes to replace a deteriorating bridge over Little Drywood Creek known as Hagar with a new bridge	*Structure	\$26,640	\$54,800 After Cost Share: <b>\$54,800</b>	\$54,800	
			Engineer	\$7,800			
			Permits				
			Site prep				
			Materials				
			Abutments	\$6,500			
			Pilings/Piers				
			Approaches				
			Culverts				
			Riprap				
			Labor	\$13,860			
			Equip Rental				
			Other				
			# of New Mi	0	\$54,800		
		<b>Other funds received, requested, or committed?</b>					
		<b>Yes or No:</b>	No	<b>Amount:</b>			
		Public or Private:	Private	Bridge Inventory #:	<b>11</b>		
		Variance or Exception?	Old Bridge Size:	New Bridge Size:	44' x 12'		
			Water Body:	Labor Source:	Contract		
			Little Drywood	Design Weight Load:	25,000		
			Years in System:				
<b>#15</b>	WCR-N Dunn	Dunn Co proposes to replace a deteriorated bridge over Knights Creek known as Pax with a new bridge. This is a heavily used trail.	*Structure	\$44,000	\$68,200 After Cost Share: <b>\$68,200</b>	\$68,200	
			Engineer	\$4,000			
			Permits	\$200			
			Site prep	\$5,000			
			Materials				
			Abutments	\$7,000			
			Pilings/Piers				
			Approaches				
			Culverts				
			Riprap	\$2,000			
			Labor	\$5,000			
			Equip Rental				
			Other	\$1,000			
			# of New Mi	0	\$68,200		
		<b>Other funds received, requested, or committed?</b>					
		<b>Yes or No:</b>	No	<b>Amount:</b>			
		Public or Private:	Private	Bridge Inventory #:	<b>Dunn-10</b>		
		Variance or Exception?	Old Bridge Size:	New Bridge Size:	50' x 12'		
			Water Body:	Labor Source:	Contract		
			Knights Creek	Design Weight Load:	25,000		
			Years in System:				

## 2021-22 SNOWMOBILE BRIDGE REHABILITATION REQUESTS

Region	County		Component	Costs	Total	Approved	Q & A
<b>#16</b>	Dunn	Dunn Co proposes to replace a bridge over the Eau Galle River. The approaches and deck are rotting. This bridge is part of a mandatory reroute which will remove a portion of trail located in the floodplain. No development costs are being requested for the reroute.	*Structure	\$85,000	\$120,713 After Cost Share: <b>\$120,713</b>	\$120,713	
			Engineer	\$8,000			
			Permits	\$200			
			Site prep	\$5,000			
			Materials				
Abutments	\$10,000						
			Pilings/Piers				
			Approaches				
			Culverts				
			Riprap	\$2,000			
			Labor	\$10,000			
			Equip Rental				
			Other	\$513			
			# of New Mi	0	\$120,713		
		<b>Other funds received, requested, or committed?</b>					
		<b>Yes or No:</b>	No	<b>Amount:</b>			
		Public or Private:	Private	Bridge Inventory #:	Dunn-25		
		Old Bridge Size:	47' x 12'	New Bridge Size:	90' x 12'		
		Water Body:	Eau Galle River	Labor Source:	Contract		
		Years in System:	40+	Design Weight Load:	25,000		
<b>#17</b>	Dunn	Dunn Co proposes to replace a bridge over Gilbert Creek known as Holte Bridge. Due to heavy rains, flooding and erosion, the bridge is no longer located on top of the bank. The bridge was closed and the trail rerouted, but the reroute is dangerous.	*Structure	\$44,000	\$68,200 After Cost Share: <b>\$68,200</b>	\$68,200	
			Engineer	\$4,000			
			Permits	\$200			
			Site prep	\$5,000			
			Materials				
Abutments	\$7,000						
			Pilings/Piers				
			Approaches				
			Culverts				
			Riprap	\$2,000			
			Labor	\$5,000			
			Equip Rental				
			Other	\$1,000			
			# of New Mi	0	\$68,200		
		<b>Other funds received, requested, or committed?</b>					
		<b>Yes or No:</b>	No	<b>Amount:</b>			
		Public or Private:	Private	Bridge Inventory #:	Dunn-22		
		Old Bridge Size:	40' x 12'	New Bridge Size:	50' x 12'		
		Water Body:	Gilbert Creek	Labor Source:	Contract		
		Years in System:	20+	Design Weight Load:	25,000		
<b>#18</b>	Eau Claire	Eau Claire Co proposes to rehabilitate a bridge over the Eau Claire River known as Troubled Waters Bridge. This bridge is approx. 16' X 202' and was previously an old highway bridge. Inspection determined the bridge to be in need of repair. Piles are completely rusted through and will need to be repaired or replaced. Application is split with ATV.	*Structure		\$129,875 After Cost Share: <b>\$64,938</b>	\$64,938	Redo cost estimate without contingency. <i>Done</i>
			Engineer	\$1,600			
			Permits				
			Site prep	\$8,000			
			Materials				
Abutments							
			Pilings/Piers	\$83,181			
			Approaches	\$24,044			
			Culverts				
			Riprap				
			Labor				
			Equip Rental				
			Other	\$23,365			
			# of New Mi	0	\$64,938		
		<b>Other funds received, requested, or committed?</b>					
		<b>Yes or No:</b>	Yes	<b>Amount:</b>	\$64,937.50		
		Public or Private:	Public	Bridge Inventory #:	16		
		Old Bridge Size:	16' x 202'	New Bridge Size:	16' x 202'		
		Water Body:	Eau Claire River	Labor Source:	Contract		
		Years in System:	70+	Design Weight Load:	25,000		

## 2021-22 SNOWMOBILE BRIDGE REHABILITATION REQUESTS

Region	County		Component	Costs	Total	Approved	Q & A
<b>#19</b>	Eau Claire	Eau Claire Co proposes to replace a bridge over Otter Creek known as Foster. The existing bridge was club built bridge. A more permanent type structure is necessary.	*Structure	\$63,700	\$80,500 After Cost Share: <b>\$80,500</b>	\$80,500	
			Engineer	\$3,800			
			Permits				
			Site prep	\$10,000			
			Materials				
<b>Other funds received, requested, or committed?</b>							
<b>Yes or No:</b> <b>No</b> <b>Amount:</b> <b></b>							
Variance or Exception?	Public or Private:	Private	Bridge Inventory #:	<b>22</b>			
	Old Bridge Size:	10' x 20'	New Bridge Size:	12' x 34' w appr			
	Water Body:	Otter Creek	Labor Source:	Contract			
	Years in System:	4	Design Weight Load:	25,000			
			# of New Mi	0	\$80,500		
<b>#20</b>	Eau Claire	Eau Claire Co proposes to replace a bridge over Beaver Creek known as Brackett Bridge. The existing club built bridge was built over 30 years ago and needs to be replaced. Significant amounts of high water and flooding have eroded the bank and weakened the structure making it unsafe.	*Structure	\$63,700	\$82,500 After Cost Share: <b>\$82,500</b>	\$82,500	
			Engineer	\$3,800			
			Permits				
			Site prep	\$10,000			
			Materials				
<b>Other funds received, requested, or committed?</b>							
<b>Yes or No:</b> <b>No</b> <b>Amount:</b> <b></b>							
Variance or Exception?	Public or Private:	Private	Bridge Inventory #:	<b>7</b>			
	Old Bridge Size:	10' x 30'	New Bridge Size:	12' x 34' w appr			
	Water Body:	Beaver Creek	Labor Source:	Contract			
	Years in System:	30 +	Design Weight Load:	25,000			
			# of New Mi	0	\$82,500		
<b>#21</b>	Jackson	Jackson Co proposes to replace a bridge over Douglas Creek known as Douglas Creek Bridge. Several storm events have caused the banks to widen and weakened the bridge structure. Has Chapter 30 permit.	*Structure	\$54,000	\$109,000 After Cost Share: <b>\$109,000</b>	\$109,000	
			Engineer	\$3,000			
			Permits				
			Site prep	\$1,000			
			Materials				
<b>Other funds received, requested, or committed?</b>							
<b>Yes or No:</b> <b>No</b> <b>Amount:</b> <b></b>							
Variance or Exception?	Public or Private:	Private	Bridge Inventory #:	<b>341865</b>			
	Old Bridge Size:		New Bridge Size:	12' x 105'			
	Water Body:	Douglas Creek	Labor Source:	Contract			
	Years in System:	23	Design Weight Load:	25,000			
			# of New Mi	0	\$109,000		

## 2021-22 SNOWMOBILE BRIDGE REHABILITATION REQUESTS

Region	County		Component	Costs	Total	Approved	Q & A
<b>#22</b>	Pierce	Pierce Co proposes to replace a bridge over Mines Creek known as Spring Valley Bridge. The bridge is on an old railroad grade and the concrete deck is cracked and bowed.	*Structure	\$32,000	\$56,165 After Cost Share: <b>\$56,165</b>	\$56,165	
			Engineer	\$6,500			
			Permits				
			Site prep	\$1,000			
			Materials				
Abutments							
Pilings/Piers							
Approaches							
Culverts							
Riprap	\$2,000						
Labor	\$7,665						
Equip Rental							
Other	\$7,000						
<b>Other funds received, requested, or committed?</b>							
<b>Yes or No:</b> <input type="checkbox"/> No <input type="checkbox"/> Amount:							
Public or Private:		Private	Bridge Inventory #:	<b>8</b>			
Old Bridge Size:			New Bridge Size:	12' x 35.5'			
Water Body:		Mines Creek	Labor Source:				
Years in System:		35	Design Weight Load:	25,000			
Variance or Exception?			# of New Mi	0	\$56,165		
<b>#23</b>	Pierce	Pierce Co proposes to rehabilitate a bridge over Trimble River known as Gas Lite Bridge. The deck will be replaced.	*Structure		\$6,000 After Cost Share: <b>\$6,000</b>	\$6,000	
			Engineer				
			Permits				
			Site prep				
			Materials	\$4,631			
Abutments							
Pilings/Piers							
Approaches							
Culverts							
Riprap							
Labor	\$1,369						
Equip Rental							
Other							
<b>Other funds received, requested, or committed?</b>							
<b>Yes or No:</b> <input type="checkbox"/> No <input type="checkbox"/> Amount:							
Public or Private:		Private	Bridge Inventory #:	<b>1</b>			
Old Bridge Size:		12' x 96'	New Bridge Size:				
Water Body:		Trimble River	Labor Source:	Club			
Years in System:		19	Design Weight Load:	12,000			
Variance or Exception?			# of New Mi	0	\$6,000		
<b>#24</b>	Pierce	Pierce Co proposes to rehabilitate a bridge over Trimble River known as Pechacek Co W Bridge. The deck will be replaced.	*Structure	\$6,352	\$6,700 After Cost Share: <b>\$6,700</b>	\$6,700	
			Engineer				
			Permits				
			Site prep				
			Materials				
Abutments							
Pilings/Piers							
Approaches							
Culverts							
Riprap							
Labor	\$349						
Equip Rental							
Other							
<b>Other funds received, requested, or committed?</b>							
<b>Yes or No:</b> <input type="checkbox"/> No <input type="checkbox"/> Amount:							
Public or Private:		Private	Bridge Inventory #:	<b>19</b>			
Old Bridge Size:			New Bridge Size:				
Water Body:		Trimble River	Labor Source:	Club			
Years in System:		25	Design Weight Load:	12,000			
Variance or Exception?			# of New Mi	0	\$6,700		

## 2021-22 SNOWMOBILE BRIDGE REHABILITATION REQUESTS

Region	County		Component	Costs	Total	Approved	Q & A
<b>#25</b>	Trempealeau	Trempealeau Co proposes to replace a bridge over Chimney Rock Creek known as Elk Creek Bridge. The bridge originally built to move farm equipment is rotting away and for safety concerns need to be replaced.	*Structure	\$44,000	\$67,000 After Cost Share: <b>\$67,000</b>		<i>Need detailed cost estimate.</i>
			Engineer	\$4,000			
			Permits				
			Site prep	\$5,000			
			Materials				
Abutments	\$7,000						
<b>Other funds received, requested, or committed?</b>			Pilings/Piers				
<b>Yes or No:</b> No <b>Amount:</b>			Approaches				
Variance or Exception?		Public or Private: Private	Bridge Inventory #:	Riprap	\$2,000		
		Old Bridge Size: 12' x 48'	New Bridge Size: 12' x 50'	Labor	\$5,000		
		Water Body: Chimney Rock	Labor Source: Contract	Equip Rental			
		Years in System: 40+	Design Weight Load: 25,000	Other			
				# of New Mi	0	\$67,000	
<b>#26</b>	Trempealeau	Trempealeau Co proposes to replace a bridge over Lakes Coulee Creek known as Blair Bridge.	*Structure	\$58,000	\$81,000 After Cost Share: <b>\$81,000</b>		<i>Need detailed cost estimate. Double check length of LUA going forward.</i>
			Engineer	\$3,000			
			Permits				
			Site prep	\$1,000			
			Materials				
Abutments	\$12,000						
<b>Other funds received, requested, or committed?</b>			Pilings/Piers				
<b>Yes or No:</b> No <b>Amount:</b>			Approaches				
Variance or Exception?		Public or Private: Private	Bridge Inventory #:	Culverts			
		Old Bridge Size: 12' x 50'	New Bridge Size: 12' x 65'	Riprap	\$2,000		
		Water Body: Lakes Coulee Cr	Labor Source: Contract	Labor	\$5,000		
		Years in System: 40+	Design Weight Load: 25,000	Equip Rental			
				Other			
			# of New Mi	0	\$81,000		
<b>#27</b>	Juneau	Juneau Co requests funding to retrofit an existing railroad bridge in Elroy that serves as a link to the Omaha, 400, and Elroy-Sparta trails. The existing decking is no longer safe for snowmobiles or groomers and failure to repair would result in significant reroutes - County is working on obtaining a 2nd quote (05/18/21)	*Structure	\$103,000	\$103,000 After Cost Share: <b>\$103,000</b>	\$103,000	<i>Need 2nd estimate. Original estimate is structure only, who is doing the install? How much?</i>  <i>2nd estimate added along with install costs on 1st est.</i>
			Engineer				
			Permits				
			Site prep				
			Materials				
Abutments							
<b>Other funds received, requested, or committed?</b>			Pilings/Piers				
<b>Yes or No:</b> Public <b>Amount:</b>			Approaches				
Variance or Exception?		Public or Private: Public	Bridge Inventory #:	Culverts			
		Old Culvert Size:	New Culvert Size:	Riprap			
		Water Body:	Labor Source:	Labor			
		Years in System:	Design Weight Load: 25,000	Equip Rental			
				Other			
			# of New Mi	0	\$103,000		

## 2021-22 SNOWMOBILE BRIDGE REHABILITATION REQUESTS

Region	County		Component	Costs	Total	Approved	Q & A
<b>#28</b>	Wood	Replacement of wood decking on bridge over the Little Hemlock creek. Groomer has gone thru the deck and bridge is now unuseable.	*Structure	\$40,000	\$40,000 After Cost Share: <b>\$40,000</b>	\$40,000	
			Engineer				
			Permits				
			Site prep				
<b>Other funds received, requested, or committed?</b>							
<b>Yes or No:</b>			<b>Amount:</b>				
Public or Private:		Private	Bridge Inventory #:				
Old Culvert Size:			New Culvert Size:				
Water Body:		Little Hemlock	Labor Source:				
Years in System:		17	Design Weight Load: 14,000				
Variance or Exception?			# of New Mi	0	\$40,000		
<b>#29</b>	Wood	Replace the existing Auburndale Nite Owl bridge with a 60' clear span self weathering steel structure.	Bridge 1	\$56,900	\$182,900 After Cost Share: <b>\$182,900</b>	\$0	Appendix A incomplete. Length of agreement? (1 year at the moment) Load? #'s don't line up with the quote from Anderson in application. (No response) Not a valid estimate
			Bridge 2				
			Permits				
			Site prep	\$20,000			
<b>Other funds received, requested, or committed?</b>							
<b>Yes or No:</b>			<b>Amount:</b>				
Public or Private:		Private	Bridge Inventory #:				
Old Bridge Size:			New Bridge Size: 12x60				
Water Body:		N Fork Hemlock	Labor Source:				
Years in System:			Design Weight Load: <b>25,000</b>				
Variance or Exception?			# of New Mi	0	\$182,900		
<b>#30</b>	Monroe	Replacement of the Trail 27 Norwalk Trail Riders bridge. Existing structure is 25+ years old and in disrepair due to floating off during high water events. New bridge will be anchored on 1 side and fre on the other to allow it to move but not float off. Actual contract to be open bid, 2 estimates provided.	Bridge 1	\$36,000	\$37,300 After Cost Share: <b>\$37,300</b>	\$37,300	Length of easement going FWD?  Contingent on 3 year LUA or county vouching that they will reimburse the program if the permission is lost
			Bridge 2				
			Permits				
			Site prep				
<b>Other funds received, requested, or committed?</b>							
<b>Yes or No:</b>			<b>Amount:</b>				
Public or Private:		Private	Bridge Inventory #:				
Old Bridge Size:		12x40	New Bridge Size: 12x40				
Water Body:		Moore Creek	Labor Source:				
Years in System:		25	Design Weight Load: <b>25,000</b>				
Variance or Exception?			# of New Mi	0	\$37,300		

## 2021-22 SNOWMOBILE BRIDGE REHABILITATION REQUESTS

Region	County		Component	Costs	Total	Approved	Q & A		
<b>#31</b>	SCR	Columbia	HWY 146 Bridge - Request to replace existing bridge which is in poor condition. Bridge over an unnamed creek is failing, unusable, and does not comply with the new bridge guidelines. By replacing, snowmobiles can safely cross the creek by the bridge instead of having to route the snowmobile trail onto State Hwy 146 to go around the old bridge. Trail has been established on this land for the last 20+ years  <b>Other funds received, requested, or committed?</b> <b>Yes or No:</b> <input type="checkbox"/> No <b>Amount:</b> <input type="text"/> Public or Private: Private Bridge Inventory #: <input type="text"/> Old Bridge Size: 7x40 New Bridge Size: 12x40 Water Body: unnamed Labor Source: <input type="text"/> Years in System: <input type="text"/> Design Weight Load: <b>25,000</b>	Bridge 1 Bridge 2 Permits Site prep Materials Abutments Pilings/Piers Approaches Culverts Riprap Labor Equip Rental Other	\$42,200              \$9,400	\$51,600 After Cost Share: <b>\$51,600</b>	\$51,600	Is this a funded trail? What is the length of the easement going FWD?  Yes, funded and LUA is 10 years, not 30, Tail has been around for 30+	
		Variance or Exception?							
<b>#33</b>	SCR	Dane	Request to replace Dane6/MSD2 bridge in the Village of Marshal on State Corridor #36. Current bridge is 18 years old, of aging wood pole construction and unknown weight capacity. The new bridge would conform to all state waterway regulations and DNR load requirements.  <b>Other funds received, requested, or committed?</b> <b>Yes or No:</b> <input type="checkbox"/> No <b>Amount:</b> <input type="text"/> Public or Private: Private Bridge Inventory #: <input type="text"/> Old Bridge Size: 14x20 New Bridge Size: 12x38 Water Body: unnamed Labor Source: <input type="text"/> Years in System: <input type="text"/> Design Weight Load: <b>25,000</b>	Bridge 1 Bridge 2 Permits Site prep Materials Abutments Pilings/Piers Approaches Culverts Riprap Labor Equip Rental Install/Remove	\$57,815              \$7,400       \$29,850	\$95,065 After Cost Share: <b>\$95,065</b>	\$95,065	Is the LUA really 20 years?  20 year LUA is in the application packet	
		Variance or Exception?							
<b>#34</b>	SCR	Richland	The Muscoda Trail Corridor 27 bridge is deteriorating and in need of replacement. New bridge will also place the structure above the ordinary high water line  <b>Other funds received, requested, or committed?</b> <b>Yes or No:</b> <input type="checkbox"/> No <b>Amount:</b> <input type="text"/> Public or Private: Private Bridge Inventory #: <input type="text"/> Old Bridge Size: 29x12 New Bridge Size: 45x12 Water Body: Indian Creek Labor Source: <input type="text"/> Years in System: 18 Design Weight Load: <b>25,000</b>	Bridge 1 Permits Site prep   Materials Abutments Pilings/Piers Culverts Approaches Culverts Riprap Labor Equip Rental Other	\$50,000              \$10,000       \$6,000 \$2,000 \$4,000	\$68,000 After Cost Share: <b>\$68,000</b>	\$68,000	Updated estimates	
		Variance or Exception?							



## 2021-22 SNOWMOBILE BRIDGE REHABILITATION REQUESTS

Region	County		Component	Costs	Total	Approved	Q & A	
<b>#60</b>	NOR-R	Iron	Iron County proposes to replace the wear surface, deck, and railings on the Trail 13 bridge over Weber Creek. The trail is located on County Forest Property and is only used for snowmobiles.		Bridge 1	\$23,289	\$23,289	
					Permits	After Cost Share:		
					Site prep	<b>\$23,289</b>		
					Materials	\$11,117		
					Abutments			
Pilings/Piers								
Culverts								
<b>Other funds received, requested, or committed?</b>				Approaches				
		<b>Yes or No:</b>	No	<b>Amount:</b>				
Variance or Exception?		Public or Private:	Public	Bridge Inventory #:	<b>Iron Co 18</b>	Riprap		
		Old Bridge Size:	20x12	New Bridge Size:	20x12	Labor	\$6,290	
		Water Body:	Weber Creek	Labor Source:	Force	Equip Rental	\$5,882	
		Years in System:	35	Design Weight Load:	<b>25,000</b>	Other		
					\$23,289			
<b>#61</b>	SCR	Jefferson	Jefferson County proposes to replace the aging bridge over Corridor 15. Current bridge is wood and failing to the point of being unsafe.		Bridge 1	\$44,000	\$57,000	
					Permits	After Cost Share:		
					Site prep	<b>\$57,000</b>		
					Materials	\$1,000		
					Abutments	\$3,000		
Pilings/Piers								
Culverts								
<b>Other funds received, requested, or committed?</b>				Approaches				
		<b>Yes or No:</b>	No	<b>Amount:</b>				
Variance or Exception?		Public or Private:	Private	Bridge Inventory #:		Riprap	\$2,000	
		Old Bridge Size:	16x20	New Bridge Size:	40x12	Labor	\$4,000	
		Water Body:	unnamed	Labor Source:		Equip Rental		
		Years in System:	10	Design Weight Load:	<b>25,000</b>	Other	\$3,000	
					\$57,000			
<b>#62</b>	SCR	Jefferson	Jefferson County proposes to replace the aging bridge over Corridor 9 over and unnamed drainage ditch. Current bridge is wood and in need of replacement or extensive repairs		Bridge 1	\$37,000	\$50,000	
					Permits	After Cost Share:		
					Site prep	<b>\$50,000</b>		
					Materials	\$1,000		
					Abutments	\$3,000		
Pilings/Piers								
Culverts								
<b>Other funds received, requested, or committed?</b>				Approaches				
		<b>Yes or No:</b>	No	<b>Amount:</b>				
Variance or Exception?		Public or Private:	Private	Bridge Inventory #:		Riprap	\$2,000	
		Old Bridge Size:	unnamed	New Bridge Size:	34x12	Labor	\$4,000	
		Water Body:	unnamed	Labor Source:		Equip Rental		
		Years in System:		Design Weight Load:	<b>25,000</b>	Other	\$3,000	
					\$50,000			

## 2021-22 SNOWMOBILE BRIDGE REHABILITATION REQUESTS

Region	County		Component	Costs	Total	Approved	Q & A
				<b>TOTAL</b>	<b>\$2,203,543</b>	\$1,712,294	\$0

## 2021-22 SNOWMOBILE TRAIL REHABILITATION REQUESTS

Region	County	Description	Component	Costs	Total	Approved	Comments
NOR-R	Vilas	Vilas County proposes to rehab about 0.5 mile of funded snowmobile trail from CTH E to Tower Rd. Work will repair the trail from significant erosion and modify grades to help reduce future issues. This trail is also open to ATVs. The project is proposed to be split with ATV funds.	*Structure		\$7,163	\$3,581	
			Engineer		After Cost Share:		
			Permits		<b>\$3,581</b>		
			Site prep				
			Materials	\$4,303			
Abutments							
Pilings/Piers							
Approaches							
Culverts							
Riprap							
Labor							
Equip Rental	\$2,860						
Other							
		<b>Other funds received, requested, or committed?</b>					
		<b>Yes or No:</b>	<b>Yes</b>	<b>Amount:</b>	<b>\$</b>	<b>3.582</b>	
Public or Private:		Private	Bridge Inventory #:	N/A			
Old Bridge Size:		N/A	New Bridge Size:	N/A			
Water Body:		N/A	Labor Source:	Contract			
Years in System:			Design Weight Load:	N/A			
Variance or Exception?			# of New Mi	0	\$3,581		
Yes							
NOR-R	Vilas	Vilas County proposes to rehab about 3.5 miles of Snow Trail 13 in the Town of Conover, from CTH K to Big Portage Lake Rd. Work will include adding material to raise low sections, spot treating problem areas with large material, installing diversion ditches and culverts for water control, and installing 6 inches of gravel over the top of the full section. There is one bridge on this section, but no rehab funds are requested for it at this time. The County has tentatively been awarded RTP funds and is also applying for ATV grant funds.	*Structure		\$37,545	\$9,386	Consider moving gravel to ATV Program, not Snow. -- Vilas Co has revised the cost to the critical areas, as well as further justification. This was added to their application.
			Engineer		After Cost Share:		
			Permits		<b>\$9,386</b>		
			Site prep				
			Materials	\$26,240			
Abutments							
Pilings/Piers							
Approaches							
Culverts	\$250						
Riprap							
Labor							
Equip Rental	\$11,055						
Other							
		<b>Other funds received, requested, or committed?</b>					
		<b>Yes or No:</b>	<b>Yes</b>	<b>Amount:</b>	<b>\$</b>	<b>28.159</b>	
Public or Private:		Public	Bridge Inventory #:	N/A			
Old Bridge Size:		N/A	New Bridge Size:	N/A			
Water Body:		N/A	Labor Source:	Contract			
Years in System:			Design Weight Load:	N/A			
Variance or Exception?			# of New Mi		\$9,386		
No							
NOR-R	Florence	Florence County proposes to rehab a portion of Corridor Trail 6, west of Sand Lake Road. The project will address about 3.5 miles of trail and will consist of spot placement of larger ore to repair hills, turns, and low areas. The county will also place additional gravel to portions of the trail (6 inches). Then, the entire trail will be regraded, with gravel reclaimed as much as possible, to shape and re-crown the full distance. The county has been tentatively awarded RTP funds and is also applying for ATV funds for this project.	*Structure		\$34,358	\$8,590	Florence Co indicates this is necessary to keep a good base for snow grooming, especially in low snow years.
			Engineer		After Cost Share:		
			Permits		<b>\$8,590</b>		
			Site prep				
			Materials	\$10,580			
Abutments							
Pilings/Piers							
Approaches							
Culverts							
Riprap	\$1,280						
Labor	\$7,637						
Equip Rental	\$14,862						
Other							
		<b>Other funds received, requested, or committed?</b>					
		<b>Yes or No:</b>	<b>Yes</b>	<b>Amount:</b>	<b>\$</b>	<b>25.769</b>	
Public or Private:		Public	Bridge Inventory #:	N/A			
Old Bridge Size:		N/A	New Bridge Size:	N/A			
Water Body:		N/A	Labor Source:	Contract			
Years in System:			Design Weight Load:	N/A			
Variance or Exception?			# of New Mi	0	\$8,590		
No							

## 2021-22 SNOWMOBILE TRAIL REHABILITATION REQUESTS

Region	County	Description	Component	Costs	Total	Approved	Comments
NER	Oconto	Oconto County proposes to rehab the entrance of Location 657, USFS trail 2237A, to make safe riding conditions by covering boulders with gravel. The county will also add gates to prevent further damage by off road 4x4 vehicles illegally riding the snowmobile trail. These vehicles have wrecked the trail base leaving large boulders exposed. Gravel is needed to fill back in the ruts and build the base back over the boulders to allow for safe travel by snowmobilers. These boulders can be the size of a Lincoln Town Car, so easier to cover up with gravel than to remove them. RTP awarded. *County interprets the 7 mile length of the trail segment qualify this request.	*Structure		\$16,780	\$8,390	
			Engineer		After Cost Share:		
			Permits		<b>\$8,390</b>		
			Site prep				
			Materials	\$13,180			
Abutments							
Pilings/Piers							
Approaches							
Culverts							
Riprap							
Labor							
Equip Rental							
Gates	\$3,600						
			# of New Mi	0	\$8,390		
		<b>Other funds received, requested, or committed?</b>					
		<b>Yes or No:</b>	YES	<b>Amount:</b>	\$	<b>8.390</b>	
Variance or Exception?		Public or Private:	Public	Bridge Inventory #:			
		Old Bridge Size:	#	New Bridge Size:			
		Water Body:		Labor Source:			
Yes*		Years in System:		Design Weight Load:			
NER	Oconto	Oconto County proposes to rehab the entrance of Location 675, USFS East side 617,677,650 Loop Trail 1, to make safe riding conditions by covering boulders with gravel. The county will also add gates to prevent further damage by off road 4x4 vehicles illegally riding the snowmobile trail. These vehicles have wrecked the trail base leaving large boulders exposed. Gravel is needed to fill back in the ruts and build the base back over the boulders to allow for safe travel by snowmobilers. These boulders can be the size of a Lincoln Town Car, so easier to cover up with gravel than to remove them. RTP awarded. *County interprets the 5 mile length of the trail segment qualify this request.	*Structure		\$34,264	\$17,132	
			Engineer		After Cost Share:		
			Permits		<b>\$17,132</b>		
			Site prep				
			Materials	\$30,664			
Abutments							
Pilings/Piers							
Approaches							
Culverts							
Riprap							
Labor							
Equip Rental							
Gates	\$3,600						
			# of New Mi	0	\$17,132		
		<b>Other funds received, requested, or committed?</b>					
		<b>Yes or No:</b>	YES	<b>Amount:</b>	\$	<b>17.132</b>	
Variance or Exception?		Public or Private:	Public	Bridge Inventory #:			
		Old Bridge Size:		New Bridge Size:			
		Water Body:		Labor Source:			
Yes*		Years in System:		Design Weight Load:			
NER	Marinette	Marinette County proposes to rehab 1320ft section of Near North West 11 mile trail to repair eroded banks that are washing into the wetland. 50% ATV requested. Option 2 is rehab only the worst 300ft for a cost of \$24,500.	Excavate/Hau	\$42,000	\$86,500	\$43,250	<i>This seems more of an ORV problem. How are snowmobiles responsible for the damage? 60 degree slope of the banks has created erosion issues - not specific to either trail user.</i>
			Fabric	\$3,200	After Cost Share:		
			Tree Removal	\$3,500	<b>\$43,250</b>		
			Site prep				
			Materials				
Abutments							
Pilings/Piers							
Approaches							
Culverts							
Riprap	\$10,500						
Labor	\$3,000						
Equip Rental	\$24,300						
Other							
			# of New Mi	0	\$43,250		
		<b>Other funds received, requested, or committed?</b>					
		<b>Yes or No:</b>	Yes	<b>Amount:</b>	\$	<b>43.250</b>	
Variance or Exception?		Public or Private:	Public	Bridge Inventory #:			
		Old Bridge Size:		New Bridge Size:			
		Water Body:		Labor Source:			
Yes		Years in System:		Design Weight Load:			

## 2021-22 SNOWMOBILE TRAIL REHABILITATION REQUESTS

Region	County	Description	Component	Costs	Total	Approved	Comments
<b>#41</b>	Burnett	Burnett County proposes to rehabilitate 6.5 miles of snowmobile/Winter ATV trail known as Trail 151. The repair work will include filling in water holes and re-establishing a crown to promote water run-off from the trail surface; addressing areas prone to washouts; repairing ruts; and adding rock or gravel where necessary. Work will only be done on areas of the trail in need of repair. Requesting 50/50 split of Snowmobile & ATV. RTP applied for but not awarded.	*Structure		\$76,923	\$38,462	
			Engineer		After Cost		
			Permits		<b>\$38,462</b>		
			Site prep				
			Materials				
			Abutments				
			Pilings/Piers				
			Approaches				
			Culverts				
			Riprap				
			Labor				
			Equip Rental				
			Other	\$76,923			
			# of New Mi	0	\$38,462		
			<b>Yes or No:</b>		Yes	<b>Amount:</b> <b>\$38,462</b>	
Variance or Exception?		Public or Private:	Bridge Inventory #:				
		Old Bridge Size:	New Bridge Size:				
		Water Body:	Labor Source:				
		Years in System:	Design Weight Load:				
<b>#42</b>	Vernon	Trail 27 is in need to rehab work to widen the trail and alter the pitch to facilitate better run-off. Currently the groomer is unable to make it up the hill and must go around. Proposed work would include widening the trail, establish proper pitch mad removal of some trees and stumps	*Structure		\$6,500	\$0	<i>Need photos, cost break down</i>
			Engineer		After Cost Share:		
			Permits		<b>\$6,500</b>		
			Site prep				
			Materials				
			Abutments				
			Pilings/Piers				
			Approaches				
			Culverts				
			Riprap				
			Labor				
			Equip Rental				
			Other	\$6,500			
			# of New Mi	0	\$6,500		
			<b>Other funds received, requested, or committed?</b>				
			<b>Yes or No:</b>		Private	<b>Amount:</b>	
Variance or Exception?		Public or Private:	Bridge Inventory #:				
		Old Bridge Size:	New Bridge Size:				
		Water Body:	Labor Source:				
		Years in System:	Design Weight Load:				
<b>#43</b>	Richland	Pine River Corridor #32 within the city limits of Richland Center has experienced significant erosion due to recent flooding. The streembank is destabilizing and the trail is in danger of becoming too narrow to accommodate the groomer. If not addressed, the trail will mneed to be rerouted, which will possibly include bridge work. Length of trail needing work is approximately 400'	*Structure		\$14,000	\$14,000	<i>Photos don't show the need, need better photos.</i>  <i>Additional photos added</i>
			Engineer		After Cost Share:		
			Permits		<b>\$14,000</b>		
			Site prep				
			Materials				
			Abutments				
			Pilings/Piers				
			Approaches				
			Culverts				
			Riprap				
			Labor				
			Equip Rental				
			Other	\$14,000			
			# of New Mi	0	\$14,000		
			<b>Other funds received, requested, or committed?</b>				
			<b>Yes or No:</b>		no	<b>Amount:</b>	
Variance or Exception?		Public or Private:	Bridge Inventory #:				
		Old Bridge Size:	New Bridge Size:				
		Water Body:	Labor Source:				
		Years in System:	Design Weight Load:				

## 2021-22 SNOWMOBILE TRAIL REHABILITATION REQUESTS

Region	County	Description	Component	Costs	Total	Approved	Comments	
<b>#44</b>	SCR	Richland	Pine River Corridor #32 on the Schreibers property has experienced significant erosion due to recent flooding. The streembank is destabilizing and the trail is in danger of becoming too narrow to accommodate the groomer. If not addressed, the trail will mneed to be rerouted, which will possibly include bridge work. Lenght of trail needing work is approximately 750'			\$26,250	\$26,250	Better photos?
						After Cost Share:		
						<b>\$26,250</b>		Additional photos added
			<b>Other funds received, requested, or committed?</b>					
			<b>Yes or No:</b> no <b>Amount:</b>					
		Public or Private: Private	Bridge Inventory #:					
		Old Bridge Size:	New Bridge Size:					
		Water Body:	Labor Source:					
		Years in System:	Design Weight Load:					
			# of New Mi	0	\$26,250			
<b>#45</b>	SCR	Sauk	Trail NC7N in White Mount County Park is in need to erision repair & control. Due to severe erosion, the groomer is unable to make it up the hill, and the condition of the trail presents a safety concern for the snowmobilers. No alternative route is available as it would present the same issues on another hill. Lenght of trail is approximately 1,400.			\$40,000	\$40,000	
						After Cost Share:		
						<b>\$40,000</b>		
			<b>Other funds received, requested, or committed?</b>					
			<b>Yes or No:</b> No <b>Amount:</b>					
		Public or Private: Public	Bridge Inventory #:					
		Old Bridge Size:	New Bridge Size:					
		Water Body:	Labor Source:					
		Years in System:	Design Weight Load:					
			# of New Mi	0	\$40,000			
<b>TOTAL</b>						<b>\$215,541</b>	<b>\$209,041</b>	

## 2021-22 SNOWMOBILE MANDATORY TRAIL RELOCATION REQUESTS

Region	County	Description	Component	Costs	Total	Approved	Comments	
#46	Price	Price County proposes to reroute a portion of Trail 80, due to loss of easement from a private landowner. The former trail was about 2.5 miles in length. The new proposed trail will be about 2.2 miles in length and run along an overhead power line. The new trail will require some grading, culverts, spot fill, and signage.	*Structure		\$22,607	\$22,607	\$22,607	-- County has updated costs. They will be able to reclaim signs from the former trail. This has reduced costs by \$552.20.
			Engineer					
			Permits					
			Site prep					
			Materials	\$4,142				
Abutments								
Pilings/Piers								
Approaches								
Culverts	\$2,255							
Riprap								
Labor	\$16,210							
Equip Rental								
Other								
# of New Mi	-0.3		\$22,607					
		<b>Other funds received, requested, or committed?</b>						
		<b>Yes or No:</b>	No	<b>Amount:</b>				
Variance or Exception?		Public or Private:	Private	Bridge Inventory #:	N/A			
		Old Bridge Size:	N/A	New Bridge Size:	N/A			
		Water Body:	N/A	Labor Source:	Contract			
		Years in System:		Design Weight Load:	N/A			
#47	Waupaca	Waupaca County proposes to reroute a section of funded snowmobile trail due to lost landowner permission. The development cost is for equipment to clear the trail.	*Structure		\$5,000	\$5,000	\$5,000	
			Engineer					
			Permits					
			Site prep					
			Materials					
Abutments								
Pilings/Piers								
Approaches								
Culverts								
Riprap								
Labor								
Equip Rental	\$5,000							
Other								
# of New Mi	-0.3		\$5,000					
		<b>Other funds received, requested, or committed?</b>						
		<b>Yes or No:</b>	No	<b>Amount:</b>				
Variance or Exception?		Public or Private:	Private	Bridge Inventory #:				
		Old Bridge Size:		New Bridge Size:				
		Water Body:		Labor Source:				
		Years in System:		Design Weight Load:				
				<b>TOTAL</b>	<b>\$27,607</b>	<b>\$27,607</b>		

## 2021-22 SNOWMOBILE DISCRETIONARY TRAIL RELOCATION REQUESTS

Reg	County	Description	Component	Costs	Total	Approved	Reason	LUA Length	Safety	Trail Type	Miles Lost	Other Funds	Snowfall	County Pts.	Deduct: Multi-gr	Ded: RR Xing	Incomplete App.	TOTAL	Q&A				
NOR-R	Vilas	Vilas County proposes to reroute a portion of Corridor Trail 10/17 to move it off of an existing town road route. The proposed reroute will follow an existing woods trail, but will require a new bridge be installed.	*Structure	\$22,500	\$37,397	\$38,267	5	15	5	10	10	10	5	10*	-10	-10							
			Engineer	\$900	After Cost Share:		5	12	5	10	0	0	3	10	-5					40			
			Permits	\$1,000	<b>\$37,397</b>																		
			Site prep	\$1,000																			
			Materials																				
			Abutments	\$2,250																			
			Pilings/Piers																				
			Approaches	\$2,000																			
			Culverts																				
			Riprap																				
			Labor	\$7,747																			
			Equip Rental																				
			Other																				
			# of New Mi		2.9	\$38,267																	
		<b>Other funds received, requested, or committed?</b>																					
		<b>Yes or No:</b>			No	<b>Amount:</b>																	
		Public or Private:	Private	Bridge Inventory #:	New																		
		Variance or Exception?	Old Bridge Size:	N/A	New Bridge Size:	12' x 20'																	
			Water Body:	Ila	Labor Source:	Contract																	
			Years in System:	0	Design Weight Load:	25,000																	
NOR-S	Burnett	Burnett Co requests funding to reroute three sections of Trail 8 in the Town of Swiss. These reroutes are being requested for safety reasons and to alleviate damage being caused to town roads. Total Project Cost: \$72,522. RTP applied for but not awarded. Burnett is requesting all of the gravel from ATV, but a 50/50 split on trail construction.	*Structure		\$72,522	\$6,887																	
			Engineer		After Cost Share:																		
			Permits		<b>\$6,887</b>																		
			Site prep																				
			Materials	\$58,748																			
			Abutments																				
			Pilings/Piers																				
			Approaches																				
			Culverts																				
			Riprap																				
			Labor																				
			Equip Rental	\$13,634																			
			Other	\$140																			
			# of New Mi		0	\$6,887																	
		<b>Other funds received, requested, or committed?</b>																					
		<b>Yes or No:</b>			Yes	<b>Amount:</b>	\$ 65,635																
		Variance or Exception?	Public or Private:	Public	Bridge Inventory #:																		
			Old Bridge Size:		New Bridge Size:																		
			Water Body:		Labor Source:																		
			Years in System:		Design Weight Load:																		
NOR-S	Burnett	Burnett Co requests funding to reroute a portion of Trail 7 in order to eliminate a town road for safety reasons. Eliminating this town road will be safer and make the ride much more enjoyable for snowmobiles in the winter. Total Project Cost: \$30,844. RTP applied for but not awarded. Burnett is requesting all of the gravel from ATV, but a 50/50 split on trail construction.	*Structure		\$30,844	\$2,231																	
			Engineer		After Cost Share:																		
			Permits		<b>\$2,006</b>																		
			Site prep																				
			Materials	\$26,831																			
			Abutments																				
			Pilings/Piers																				
			Approaches																				
			Culverts																				
			Riprap																				
			Labor																				
			Equip Rental	\$3,873																			
			Other	\$140																			
			# of New Mi		1	\$2,231																	
		<b>Other funds received, requested, or committed?</b>																					
		<b>Yes or No:</b>			Yes	<b>Amount:</b>	\$ 28,838																
		Variance or Exception?	Public or Private:	Public	Bridge Inventory #:																		
			Old Bridge Size:		New Bridge Size:																		
			Water Body:		Labor Source:																		
			Years in System:		Design Weight Load:																		



## 2021-22 SNOWMOBILE DISCRETIONARY TRAIL RELOCATION REQUESTS

Reg	County	Description	Component	Costs	Total	Approved	Reason	LUA Length	Safety	Trail Type	Miles Lost	Other Funds	Snowfall	County Pts.	Deduct: Multi-gr	Ded: RR Xing	Incomplete App.	TOTAL	Q&A				
							5	15	5	10	10	10	5	10*	-10	-10	-10						
NOR-S	Burnett	Burnett Co requests funding to reroute a portion of Snowmobile/ATV trail 41 in order to remove recreational vehicle traffic from a town road for safety reasons and also to move the winter trail off a portion of the Namekagon Barrens Wildlife Area, which was requested by DNR in the past. This reroute will connect users to snowmobile and ATV/UTV routes in Washburn County. Total Project Cost: \$60,522 (Development) RTP applied for but not awarded. ATV \$27,778. Snow \$2,483 + \$360. (New Snow Maintenance)	*Structure		\$60,522	\$2,843													0				
			Engineer				After Cost Share:																
			Permits				\$2,483																
			Site prep		\$55,415																		
			Materials																				
			Abutments																				
			Pilings/Piers																				
			Approaches																				
			Culverts																				
			Riprap																				
			Labor																				
			Equip Rental	\$4,967																			
			Other	\$140																			
						\$2,843																	
			# of New Mi		1																		
			<b>Other funds received, requested, or committed?</b>																				
			<b>Yes or No:</b>	Yes	<b>Amount:</b>	\$58,039																	
			Public or Private:	Public	Bridge Inventory #:																		
			Old Bridge Size:		New Bridge Size:																		
			Water Body:		Labor Source:																		
			Years in System:		Design Weight Load:																		
SCR	Columbia	Hwy P Bridge - Request to construct a new bridge over an unnamed creek. This would allow snowmobiles to safely cross the creek by the bridge instead of having to route the snowmobile trail onto CTY P to go around the creek. Trail has been established on this land for the last 20+ years with the land owner being a member of our club. Trail reroute would only be minimal.	Bridge 1	\$53,000	\$53,000	\$65,400	\$65,400												0	30 year LUA? 20 years, added to PDF			
			Bridge 2					After Cost Share:															
			Permits					\$65,400															
			Site prep																				
			Materials																				
			Abutments																				
			Pilings/Piers																				
			Approaches																				
			Culverts																				
			Riprap																				
			Labor																				
			Equip Rental																				
			Other	\$12,400																			
						\$65,400																	
			# of New Mi		0																		
			<b>Other funds received, requested, or committed?</b>																				
			<b>Yes or No:</b>		<b>Amount:</b>																		
			Public or Private:	Private	Bridge Inventory #:																		
			Old Bridge Size:	n/a	New Bridge Size:	54x12																	
			Water Body:	unnamed	Labor Source:																		
			Years in System:		Design Weight Load:	25,000																	
WCR-N	Clark	This project is a DOT project for a new highway bridge. They are proposing a recreational wing and looking for funding support. RTP funds are tentatively awarded for this project. <b>Note, request reviewed this summer included bridge only. Trail, engineering and site prep were not included but are now.</b>	Bridge 1	\$614,000	\$614,000	\$767,200	\$567,200												0				
			Engineering	\$82,200				After Cost Share:															
			Trail Approac	\$56,000				\$567,200															
			Site prep	\$15,000																			
			Materials																				
			Pilings/Piers																				
			Approaches																				
			Culverts																				
			Riprap																				
			Labor																				
			Equip Rental																				
			Other																				
						\$767,200																	
			# of New Mi		0																		

## 2021-22 SNOWMOBILE DISCRETIONARY TRAIL RELOCATION REQUESTS

Reg	County	Description	Component	Costs	Total	Approved	Reason	LUA Length	Safety	Trail Type	Miles Lost	Other Funds	Snowfall	County Pts.	Deduct: Multi-gr	Ded: RR Xing	Incomplete App.	TOTAL	Q&A		
NOR-S	Douglas	Douglas County proposes to relocate a snowmobile/winter-ATV trail crossing at Hwy 53 and B. The trail crossing would move 660 feet to the south, and reduce the number of lanes crossed from 6 (on 130' of pavement) to 4 (on 60' of pavement). Project is split with ATV	*Structure		\$89,154	\$44,577														0	
			Engineer	\$12,900	After Cost Share:																
			Permits		<b>\$44,577</b>																
			Site prep	\$17,775																	
			Materials	\$28,579																	
Abutments																					
<b>Other funds received, requested, or committed?</b>			Culverts	\$4,460																	
<b>Yes or No:</b>			Riprap																		
Yes			Labor																		
<b>Amount:</b>			Equip Rental	\$25,440																	
\$ 44,577			Other																		
Public or Private:			# of New Mi	0	\$44,577																
Old Bridge Size:																					
Water Body:																					
Years in System:																					
Design Weight Load:																					
NOR-S	Douglas	Douglas Co proposes relocating riders off of Crotte Brook Rd (a plowed road) and onto a new off-road trail. Trail 5 is a funded snowmobile trail on both ends of this segment. 6.5 miles of the trail is already built (w/o grant funds) and open, and this request would fund the remaining 3 miles of dev and award maintenance to the entire 9.5 miles of new trail. Split with Winter ATV	*Structure		\$11,090	\$8,395															0
			Engineer		After Cost Share:																
			Permits		<b>\$5,545</b>																
			Site prep																		
			Materials	\$4,300																	
Abutments																					
<b>Other funds received, requested, or committed?</b>			Culverts																		
<b>Yes or No:</b>			Riprap																		
Yes			Labor	\$640																	
<b>Amount:</b>			Equip Rental	\$6,150																	
\$ 5,545			Other																		
Public or Private:			# of New Mi	9.5	\$8,395																
Old Bridge Size:																					
Water Body:																					
Years in System:																					
Design Weight Load:																					
<b>TOTAL</b>							<b>\$735,800</b>	<b>\$735,800</b>													

## 2021-22 SNOWMOBILE NEW BRIDGE REQUESTS

Region	County	Component	Costs	Total	Approved	5	#	5	17	10	#	25	-10	-10	10	72	Q & A	
NOR-R	Vilas	Vilas County proposes to install a new bridge on Trail 17 in Eagle River. The area is swampy and there have been times the ground did not freeze, causing groomers to get stuck or the trail to be unusable. A bridge would allow the trail to remain open and be safer. Length will be 12' x 460' made up of 23 x 20-ft sections. This trail is also open to ATVs and the County has applied for ATV funds as well.	Structure \$379,000 Engineering \$37,000 Permits \$5,000 Site prep Materials Abutments \$101,500 Pilings/Piers Approaches \$20,000 Culverts Riprap Labor \$305,000 Equip Rental Other	\$847,500 After Cost Share: <b>\$423,750</b>	\$0	5	5	0	5	8	5	-5			10	33	<i>Additional information and justification for this project has been added to the application.</i>	
		<b>Other funds received, requested, or committed?</b>																
		<b>Yes or No:</b>	Yes	<b>Amount:</b>	423750													
Variance or Exception?		Public or Private:	Private	Bridge Inventory #:	New													
		Old Bridge Size:	N/A	New Bridge Size:	12' x 460'													
		Water Body:	Unnamed	Labor Source:	Contract													
		Years in System:	10+	Design Weight	25,000													
				# of New Mi	0													
NER	Door	Door County proposes to add a culvert over an unnamed creek. The past 3 years have required snowmobilers to reroute onto the shoulder of Poplar Lane due to running water year-round. <i>Withdrawn</i>	Structure Engineering Permits Site prep Materials Abutments Pilings/Piers Approaches Culverts \$5,000 Riprap Labor \$2,395 Equip Rental Other	\$7,395 After Cost Share: <b>\$7,395</b>	\$0												0	<i>Withdrawn</i>
		<b>Other funds received, requested, or committed?</b>																
		<b>Yes or No:</b>	no	<b>Amount:</b>														
Variance or Exception?		Public or Private:	private	Bridge Inventory #:	TBD													
		Old Bridge Size:	N/A	New Culvert Size:	60" by 18'?													
		Water Body:	Unnamed	Labor Source:	contractor													
		Years in System:	N/A	Design Weight	25,000													
				# of New Mi	0													
<b>TOTAL</b>					<b>\$423,750</b>													<b>\$0</b>



## 2021-22 NEW MILES

1. Columns E, F, & G are gateway questions and one of the columns needs to be marked for the application to be eligible.
2. Column H indicates if the required map was submitted.
3. Column I indicates if the certification statement was signed.
4. Columns J through R are the actual ratings and rankings with point value.
5. Column P thru R should be entered as consecutive numbers "1"

**Q1 - LUA Pnts Color Key**  
**GREEN** = Q 1 Pnts Verified  
**YELLOW** = Documentation requested last week, give more time  
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					1	2	3	1	2	3	1	2	3																		
NM-43	NER	Waupaca	#2 Int 38-New London/Out	4.7	x	x	x	Y	Y	0	3	1	0	1	2									7	7	\$0	\$1,410	\$1,410	connects to Outaga	n/a	
NM-28	NOR	Price	Trail 118	6.5		x	x	x	x	1	1	3	2	2	2	0								8	7	\$0	\$1,950	\$1,950			
NM-130	NOR	Sawyer	Trail 18 North	3.2	X	X	X	Y	Y	2	1	2	3	1	0	0	0	0	0	0	0	0	-2	7	7	\$0	\$963	\$963			
NM-132	NOR	Sawyer	Trail 18 South	2.0	X	X	X	Y	Y	2	1	2	3	1	0	0	0	0	0	0	0	0	-2	7	7	\$0	\$612	\$612			
NM-46	NOR	Taylor	Brooks Trail	0.6			x	x	x	0	1	2	2	2	0	0	0	0	0	0	0	0	0	7	7	\$0	\$180	\$180		n/a	
NM-47	NOR	Taylor	Rib Lake City Trail	0.4		x	x	x	x	0	1	2	2	2	0	0	0	0	0	0	0	0	0	7	7	\$0	\$120	\$120		n/a	
NM-48	NOR	Taylor	M4 Trail	0.5		x	x	x	x	0	1	2	2	2	0	0	0	0	0	0	0	0	0	7	7	\$0	\$150	\$150		n/a	
NM-49	SCR	Columbia	Trail 71	6.0	X	X	X	X	X	0	3	1	2	2	2	0							-3	0	7	7	\$0	\$1,800	\$1,800		n/a
NM-52	SCR	Dodge	Segment 8	7.4			X	X	X	2	1	1	2	2	2	0							-3	0	7	7	\$0	\$2,211	\$2,211	Added 2 pref points	Docs submitte
NM-31	SCR	Grant	Lancaster	15.0	X			X	X	1	2	1	2	1	1	0	0	0	0	0	0	0	0	8	7	\$0	\$4,500	\$4,500		Yes	
NM-53	WCR	Chippewa	Old Abe South (St. Trail)	5.9		X	X	Y		3	1	2	2	2	0								-3	7	7	\$0	\$1,770	\$1,770		Yes	
NM-54	WCR	Chippewa	Trail 64	3.5			X	Y		2	1	2	2	2	0								-2	7	7	\$0	\$1,050	\$1,050			
NM-55	WCR	Portage	Trail 406	2.4			X	X	X	0	3	1	2	2	2	0							-3	0	7	7	\$0	\$720	\$720		n/a
NM-20	NER	Calumet	Badger Creek Trail	2.4	x			Y	Y	1	3	1	1	2	2								-3	7	6	\$0	\$720	\$720	connects 2020 funded	Manitowoc	
NM-57	NER	Kewaunee	L-ELL	8.1			x	Y	Y	0	2	1	0	1	2								0	6	6	\$0	\$2,430	\$2,430	connects funded tra	n/a	
NM-23	NER	Manitowoc	C-2 #15	2.4	x			Y	Y	2	1	1	2	2										8	6	\$0	\$720	\$720	Not on GIS		
NM-25	NER	Marinette	#3 East side Peshtigo to Int	2.8	x			Y	Y	2	1	2	2	2	1								-2	8	6	\$0	\$840	\$840		oral easemen	
NM-58	NER	Oconto	Lower Loop	1.6			x	Y	Y	3	1	2	1	2									-3	6	6	\$0	\$480	\$480	per Geoviewer	unplowed roa	
NM-60	NER	Waupaca	#1 ShawCo Line-Emb VE 8	3.0	x	x	x	Y	Y	0	3	1	0	2										6	6	\$0	\$900	\$900		n/a	
NM-62	NOR	Burnett	Trail 22	5.3		X	X	X	X	0	1	2	3	1	2									-3	6	6	\$0	\$1,590	\$1,590		n/a
NM-63	NOR	Douglas	Trail 6	4.8			X	X	X	0	1	3	3	2	0									-3	6	6	\$0	\$1,440	\$1,440		n/a
NM-64	NOR	Price	Trail 88	2.4			x	x	x	0	1	3	2	2	0	0	0	0	0	0	0	0	-2	6	6	\$0	\$720	\$720		n/a	
NM-65	NOR	Taylor	Q Trail	3.8		x	x	x	x	0	1	2	2	2	2	0							-3	0	6	6	\$0	\$1,140	\$1,140		n/a
NM-67	NOR	Vilas	Birch Lake Trail 6 - PI	1.2		x	x	x	x	0	1	3	0	2	0	0	0	0	0	0	0	0	0	0	6	6	\$0	\$360	\$360		n/a
NM-68	NOR	Vilas	Little Bohemia	0.9			x	x	x	0	1	3	0	2	0	0	0	0	0	0	0	0	0	0	6	6	\$0	\$270	\$270		n/a
NM-70	SCR	Jefferson	Watertown By-Pass	4.5	X	X	X	X	X	0	1	1	2	1	1	0	0	0	0	0	0	0	0	0	6	6	\$0	\$1,341	\$1,341		n/a
NM-72	SCR	Sauk	HC3	7.0	X	X	X	X	X	0	1	1	3	2	2	0							-3	0	6	6	\$0	\$2,091	\$2,091		n/a
NM-133	SER	Ozaukee	Sheboygan Co Line-Hwy 51	1.8	x			Y	Y	0	3	1	1	2	2								-3	6	6	\$0	\$546	\$546	included on update	n/a	
NM-138	SER	Sheboygan	NF Club 1	3.2	x			Y	Y	0	3	1	1	1										6	6	\$0	\$972	\$972		n/a	
NM-145	WCR	Adams	R5	1.2		X		X	X	0	1	1	2	2	0	0	0	0	0	0	0	0	0	6	6	\$0	\$360	\$360		n/a	
NM-146	WCR	Adams	R6	2.7		X		X	X	0	1	1	2	2	0	0	0	0	0	0	0	0	0	6	6	\$0	\$810	\$810		n/a	
NM-73	WCR	Chippewa	Drywood Trail	1.6			X	Y		2	1	2	2	2	0								-3	6	6	\$0	\$480	\$480			
NM-74	WCR	Chippewa	Tilden Cut Across	3.8			X	Y		2	1	2	2	2	0								-3	6	6	\$0	\$1,140	\$1,140			
NM-147	WCR	Chippewa	Albertville	2.4			x	Y	x	2	1	2	2	2	0	0							-3	0	6	6	\$0	\$720	\$720		

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					1	2	3	1	2	3	1	2	3																		
NM-36	WCR	La Crosse	Hammes Trail	1.2			X	X	X	2	1	1	2	1	1	0	0	0	0	0	0	0	0	8	6	\$0	\$357	\$357		Yes	
NM-75	WCR	Pierce	County E	5.3	X		X	X	X	1	1	1	2	2	2	0	0	0	0	0	0	0	0	6	6	\$0	\$1,590	\$1,590			
NM-76	WCR	Portage	Trail 404	8.6		X	X	X	X	0	1	1	2	2	2	0	0	0	0	0	0	0	0	6	6	\$0	\$2,580	\$2,580		n/a	
NM-38	WCR	Wood	Bakerville Trail #1	1.5						2	1	1	2	2	2	0	0	0	0	0	0	0	0	8	6	\$0	\$450	\$450		Yes	
NM-24	NER	Manitowoc	D-2-2, D-2-1, B-2 Pigeon	11.5			x	Y	Y	2	1	1	2	2	2								-3	7	5	\$0	\$3,450	\$3,450		n/a	
NM-79	NER	Outagamie	Z1-1-NG	1.7	x	x	x	Y	Y	0	3	1	1	2	1								-3	5	5	\$0	\$510	\$510		n/a	
NM-66	NOR	Vilas	Trout River - LDF	1.4			x	x	x	2	1	3	0	2	0	0							-3	0	5	5	\$0	\$420	\$420		
NM-82	SCR	Columbia	Trail 86	2.1	X	X	X	X	X	0	3	1	2	2	0	0							-3	0	5	5	\$0	\$618	\$618		n/a
NM-50	SCR	Dane	Trail 61	2.1	X	X	X	X	X	2	3	1	2	1	1	0							-3	0	7	5	\$0	\$630	\$630		
NM-51	SCR	Dane	Trail 18 ( to Columbia 86)	3.9	X			X	X	2	3	1	2	1	1	0							-3	0	7	5	\$0	\$1,179	\$1,179		
NM-83	SCR	Dane	Trail 51	2.2		X	X	X	X	2	1	1	2	2	0	0							-3	0	5	5	\$0	\$660	\$660		Docs rcvd, pe
NM-84	SCR	Dodge	Segment 2	7.7		X	X	X	X	2	1	1	2	2	0	0							-3	0	5	5	\$0	\$2,306	\$2,306	<5 yet connects cor	Docs rcvd, pe
NM-85	SCR	Dodge	Segment 4	9.1		X	X	X	X	2	1	1	2	2	0	0							-3	0	5	5	\$0	\$2,730	\$2,730		Docs rcvd, pe
NM-86	SCR	Dodge	Segment 5	5.5		X	X	X	X	2	1	1	2	2	0	0							-3	0	5	5	\$0	\$1,650	\$1,650		Docs rcvd, pe
NM-87	SCR	Dodge	Segment 7	1.6		X	X	X	X	2	1	1	2	2	0	0							-3	0	5	5	\$0	\$480	\$480		Docs rcvd, pe
NM-88	SCR	Jefferson	Rome	5.2		X	X	X	X	0	1	1	2	1	0	0							0	0	5	5	\$0	\$1,566	\$1,566		n/a
NM-142	SCR	Washington	Plat Five	3.0	x			Y	Y	0	1	1	1	1	1										5	5	\$0	\$888	\$888	new GIS provided	n/a
NM-143	SER	Washington	Kettle Moraine	1.9	x	x	x	Y	Y	0	1	1	1	1	1										5	5	\$0	\$576	\$576	new GIS provided	n/a
NM-91	WCR	St. Croix	Erin to Emerald 9B	11.0			X		X	0	1	1	2	2	2								-3	5	5	\$0	\$3,300	\$3,300		n/a	
NM-56	NER	Fond du Lac	Waupun North Loop	4.8	x	x	x	Y		0	3	1	1	2									-3	4	4	\$0	\$1,440	\$1,440	connects to extra st	n/a	
NM-93	NER	Fond du Lac	Cedar Ridge	5.0	x	x	x	Y	Y	0	3	1	1	2									-3	4	4	\$0	\$1,485	\$1,485		n/a	
NM-94	NER	Kewaunee	DU-LL	1.3			x	Y	Y	0	1	1	0	2									0	4	4	\$0	\$390	\$390	ends at service	n/a	
NM-78	NER	Marinette	#2 Corridor 18, Int 216-217	1.5			x	Y	Y	1	1	2	2	2									-3	5	4	\$0	\$450	\$450			
NM-95	NER	Shawano	Tigerton to Carolina	13.0			x	Y	Y	0	2	2	1	2									-3	4	4	\$0	\$3,900	\$3,900		n/a	
NM-96	NER	Shawano	Mattoon-Langlad Co	3.9			x	Y	Y	0	3	2	1	1									-3	4	4	\$0	\$1,170	\$1,170		n/a	
NM-97	NER	Waupaca	#6D Int 49-18 Trail FF	1.4			x	Y	Y	0	1	1	0	2										4	4	\$0	\$420	\$420		n/a	
NM-98	NER	Waushara	Tri-Co #26-#30A	8.5	x	x	x	Y	Y	0	1	1	1	2	2								-3	4	4	\$0	\$2,550	\$2,550		n/a	
NM-99	NOR	Lincoln	Beacon Trail	0.7			x	x	x	0	1	2	2	2	0	0							-3	0	4	4	\$0	\$210	\$210		n/a
NM-100	NOR	Lincoln	Alta Springs Trail	8.9			x	x	x	0	1	2	2	2	0	0							-3	0	4	4	\$0	\$2,670	\$2,670		n/a
NM-101	NOR	Lincoln	Somo Lake Trail	6.0	x	x	x	x	x	0	1	2	2	1	0	0							0	-2	4	4	\$0	\$1,800	\$1,800		n/a
NM-81	NOR	Vilas	Eagle River Trail 1	3.0		x	x	x	x	0	1	3	0	2	0	0							0	-2	4	4	\$0	\$900	\$900	Runs to parking...on water.	n/a
NM-103	SCR	Columbia	Trail 61	3.8	X	X	X	X	X	0	3	1	2	1	0	0							-3	0	4	4	\$0	\$1,137	\$1,137		n/a
NM-69	SCR	Grant	Cuba City to Dickeyville	10.4	X		X	X	X	2	2	1	2	2	0	0							-3	0	6	4	\$0	\$3,120	\$3,120		Yes
NM-71	SCR	Sauk	WM1	7.8	X	X	X	X	X	2	1	1	3	2	0	0							-3	0	6	4	\$0	\$2,328	\$2,328		

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					1	2	3	1	2	3	1	2	3																			4	5
NM-135	SER	Racine	Saller Woods	2.4	x			Y	Y	3	1	1	1	2	2										7	4	\$0	\$720	\$720				
NM-141	SER	Sheboygan	RL-Club 5	2	x			Y	Y	1	3	1	1	2											5	4	\$0	\$600	\$600	no connection shown in Wash co.	n/a		
NM-104	WCR	Dunn	Trail 1	4.7	X	X	X	X	X	0	3	1	2	1	2	1	0									4	4	\$0	\$1,410	\$1,410		n/a	
NM-105	WCR	Dunn	Trail 5	3.0			X	X	X	0	1	1	2	1	2											4	4	\$0	\$900	\$900		n/a	
NM-77	NER	Manitowoc	M-2	1.2	x			Y	Y	2	1	1	2	2												5	5	\$0	\$360	\$360		n/a	
NM-59	NER	Oconto	Wheeler Loop	2.3			x	Y	Y	0	1	2	1	2												3	3	\$0	\$690		annual renew:		
NM-106	NER	Oconto	Kelly Lake Corridor 5	4.3	x		x	Y	Y	0	1	2	1	2												3	3	\$0	\$1,290		not on GIS-on new	n/a	
NM-107	NER	Shawano	Docs	5.2			x	Y	Y	0	1	2	1	2												3	3	\$0	\$1,560			n/a	
NM-108	NER	Shawano	Shortcut	1.7			x	Y	Y	0	1	2	1	2												3	3	\$0	\$510			n/a	
NM-109	NER	Shawano	Tilleda	6.8			x	Y	Y	0	1	2	1	2												3	3	\$0	\$2,040			n/a	
NM-61	NER	Waupaca	#5 Parking Corridor 9 Rail t	0.6		x	x	Y	Y	3	1	1	0	1												6	3	\$0	\$180			n/a	
NM-110	NER	Waupaca	#3 Int 68- Waushara Co AA	2.1	x		x	Y	Y	0	3	1	0	2													3	3	\$0	\$630			n/a
NM-80	NER	Winnebago	C11	6.9		x		Y	Y	0	1	1	2	2	0												3	3	\$0	\$2,070			n/a
NM-111	NOR	Lincoln	Old Grade	3.2			x	x	x	0	1	2	2	1	0	0											3	3	\$0	\$960			n/a
NM-102	NOR	Vilas	WPS - St. Germain	2.3		x	x	x	x	0	1	3	0	2	0	0											3	3	\$0	\$690			n/a
NM-113	SCR	Columbia	Trail 48 (43 to 35 connecto	3.7	X		X	X	X	0	1	1	2	2	0	0											3	3	\$0	\$1,122			n/a
NM-114	SCR	Columbia	Trail 68	4.0		X	X	X	X	0	1	1	2	2	0	0											3	3	\$0	\$1,197			n/a
NM-115	SCR	Columbia	Trail 70	4.0		X	X	X	X	0	1	1	2	2	0	0											3	3	\$0	\$1,197			n/a
NM-116	SCR	Jefferson	Concord	5.7		X	X	X	X	0	1	1	2	1	1	0											3	3	\$0	\$1,722			n/a
NM-117	SCR	Jefferson	Driftskippers	6.1				X	X	0	1	1	2	1	0	0											3	3	\$0	\$1,833			n/a
NM-89	WCR	La Crosse	Steiger Trail	0.8			X	X	X	2	1	1	2	1	1	0											5	3	\$0	\$237			Yes
NM-37	WCR	Monroe	Trail 28	9.1			X	X	X	0	1	1	2	2	0	0											3	3	\$0	\$2,730		Need GIS	
NM-90	WCR	Monroe	Trail 60	3.1			X	X	X	0	1	1	2	2	0	0											3	3	\$0	\$930			n/a
NM-92	NER	Fond du Lac	Pickett to Corr 30	4.6	x	x	x	Y	Y	0	1	1	1	2													2	2	\$0	\$1,380			n/a
NM-118	NER	Kewaunee	KU-SLN	2.1			x	Y	Y	0	2	1	0	2													2	2	\$0	\$630			n/a
NM-119	NER	Outagamie	Z5-6-PB/Z5-7-GK	3.2		x	x	Y	Y	0	1	1	1	2	0												2	2	\$0	\$960			n/a
NM-120	NER	Outagamie	Z5-4-DT	2.7	x	x	x	Y	Y	0	1	1	1	2													2	2	\$0	\$810			n/a
NM-121	NER	Outagamie	Z5-3-DT	2.4		x	x	Y	Y	0	1	1	1	2													2	2	\$0	\$720			n/a
NM-122	NER	Waupaca	#4 Int 47a-Hwy 10 QQ	2.5		x	x	Y	Y	0	2	1	0	2													2	2	\$0	\$750			n/a
NM-123	SCR	Jefferson	Piersville	5.3		X		X	X	0	1	1	2	1	0	0											3	0	\$0	\$1,581			n/a
NM-124	SCR	Jefferson	Ixonla	6.2			X	X	X	0	1	1	2	1	0	0											3	0	\$0	\$1,866			n/a
NM-137	SER	Sheboygan	HG-Club 2	3.7	x	x	x	Y	Y	2	1	1	1	2													4	2	\$0	\$1,104			n/a
NM-139	SER	Sheboygan	BHG Club 2	2.2	x	x	x	Y	Y	0	1	1	1	2													3	2	\$0	\$648			n/a
NM-125	NER	Waupaca	#6A Int 17-18 Trail O	4.8			x	Y	Y	0	1	1	0	2													1	1	\$0	\$1,440			n/a





## 2021-22 SNOWMOBILE NON-TRAIL REQUESTS

Region	County	Description	Component	Costs	Total	Approved	Comments	
<b>#56</b>	NER	Door	Door County proposes to repave the snowmobile parking at Forestville Dam park. Parking lot is shared with horse, boat, and bike users. RTP funds have been tentatively awarded.		\$29,150	\$29,150	\$0.00	<i>Motion by Andy, 2nd Mike to deny.</i>
			<b>Other funds received, requested, or committed?</b> <b>Yes or No:</b> <input type="checkbox"/> Yes <b>Amount:</b> \$ <b>14,575</b>			After Cost Share: <b>\$14,575</b>		
	Variance or Exception?		Public or Private:	Bridge Inventory #:				
			Old Bridge Size:	New Bridge Size:				
		Water Body:	Labor Source:					
		Years in System:	Design Weight Load:	# of New Mi	0	\$14,575		
						\$14,575		
<b>#57</b>	NOR-S	Douglas	Douglas County proposes to build a 20' x 40' pavilion / warming house at the junction of trail 43 and 4. Half of the structure would be open and the other would be a warming house. A privy is also available on site. Split with ATV. Applicant is also willing to consider Motorized Stewardship at 80%.		\$55,775	\$55,775	\$0.00	<i>Motion by Gary, 2nd Leon to deny.</i>
			<b>Other funds received, requested, or committed?</b> <b>Yes or No:</b> <input type="checkbox"/> Yes <b>Amount:</b> \$ <b>27,888</b>			After Cost Share: <b>\$27,888</b>		
	Variance or Exception?		Public or Private:	Bridge Inventory #:				
			Old Bridge Size:	New Bridge Size:				
		Water Body:	Labor Source:					
		Years in System:	Design Weight Load:	# of New Mi	0	\$27,888		
						\$27,888		
<b>TOTAL</b>					<b>\$42,462.50</b>	<b>\$0.00</b>		

August 17, 2021

To: Wisconsin DNR Snowmobile Recreation Council

From: West Shore Snow Pistons – Pete Tatro, Club President

Subject: Trail groomers with wings

We are sending this letter to the Council in an effort to add input and recommendations for your upcoming August 30, 2021 meeting, in regards to trail groomers with wings.

The West Shore Snow Pistons snowmobile club maintains and grooms 26.6 miles of trail from Van Dyne South through North Fond du Lac to Eldorado in Fond du Lac County.

We use a John Deere 4455 tractor to pull our 97" wide groomer. A part of our trail system is on the Mascoutin Valley State Trail RR grade. Sections of this RR grade are narrow with steep banks. With our tractor slightly wider than our drag, it was very dangerous to groom this section when getting along the edge to pull snow into the center of the trail with the drag. We were looking for a way to safely groom our trails and saw wings on a groomer and determined this would solve our safety concerns.

Our main reasons for adding wings to our groomer:

**Safety:** We are able to not risk having the groomer tractor and/or drag slide down the side of the steep banks along the RR grade. We are also able to raise the side of wide drag to make it safer for snowmobiles to pass when meeting on a narrow trail. And we can raise the sides when going down short sections of road, so the drag does not extend out into the other lane of traffic.

**Necessity:** Several of the bridges we cross along the Mascoutin RR grade trail are only 10' wide, so foldable wings are needed in order to cross the bridges with a wider drag.

**Economic:** We are able to groom a wider trail base with fewer passes, saving on grooming hours.

Reviewing the latest Equipment and Labor Rates sheet for 2017 -18 that we had, we decided to add wings to meet the A1 specifications of 11'6" minimum width.

Hydraulically operated, 22" wide, full length wings with cutters and compaction pan were added to each side of our 97" wide groomer, giving us a total width of 11'9", exceeding the minimum width requirement.

This past season we turned in grooming hours at the A1 rate. This drew the attention of the Snowmobile Recreation Council as there are not many A1 groomers being utilized. On August 14, 2021 our groomer was inspected / audited by Bob Lang, Vice-Chair of the Snowmobile Recreation Council. At this time, we found out that the minimum width of groomer wings is 24", regardless of starting width of a groomer and how wide it ends up being. We also learned that the length of groomer is measured as the length of the frame. Our frame length is 19'3", groomer length is 19'10" to front of frame pieces used to keep from hooking trees. Due to not meeting the wing width specifications that we were not aware of, our groomer will be classified at the A3 rate, until we make the needed changes.

Recommendations to the Committee:

- Support allowing the utilization of wings on groomers. Main reasons: Safety and Economic
  - Safety: Safer for grooming in areas with narrow trails and steep banks. Wide groomers are safer to pass when wings are raised up.
  - Economic: Provides a wider trail base with fewer grooming hours.

- Economic (2): Eliminates the immediate need to have all existing narrow bridges modified or replaced to meet the current 12' width requirement.
- Modify the wing specification: Eliminate the minimum wing width of 24", and replace with a minimum drag width of 11'6" with wings down. This would allow the groomer to cross 12' wide bridges with wings down and keep drags from getting too wide. For example, currently a 9' wide drag would end being 13' wide with wings.

Observations:

With utilizing the wings on our groomer we were able to have a better trail on the RR grade and do it safer. The base was wider and well packed down. Trails across farm fields were better and wider with fewer passes. The compaction of the snow under the wings was the same as the main groomer section. We are surprised that more clubs are not utilizing wings or wider groomers where feasible.

Questions to consider:

1. Is there a published groomer recommended design specification booklet available for clubs?
2. Where did the minimum 24" wide wing specification come from? What were the design criteria used to support this width specification?
3. Bob Lang commented that he is of the opinion that wings do not compact the trail as much as the main groomer section. Has there been any studies done to measure snow compaction across the width of a trail to support this claim? In the agriculture industry soil compaction measurement is common. This same technology could be used to measure the compaction of the snow after a trail is groomed.
4. During early season grooming most clubs go out to 'pan' the trails, by raising the front of the groomer up, thus keeping the cutters from digging in and putting more weight on the back pan. Since this practice does not utilize the full minimum length of the drag, will rates for 'panning' be reduced to the A4 rate?

Conclusion:

We recommend that the utilization of wings is allowed and encouraged for the Safety and Economic reasons listed above.

We recommend to change the wing width specification to create a total minimum drag width of 11'6" with wings, eliminating the 24" minimum wing width.

We ask that this Council supports legislation that helps clubs that are doing their best with the resources available and not out to maliciously circumvent rules. As we are all aware, the number of clubs and volunteers continue to decrease. Restrictive legislation will, in the long run, reduce the number of miles of trails in the state of WI.

We look forward to the outcome of your August 30 meeting in regards to groomer wings. Afterward, we will further modify our groomer accordingly to meet the published specifications for wings.

Respectfully submitted,

Pete Tatro

President, West Shore Snow Pistons

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